



AutoZone Store Development
Plan Submission
for:

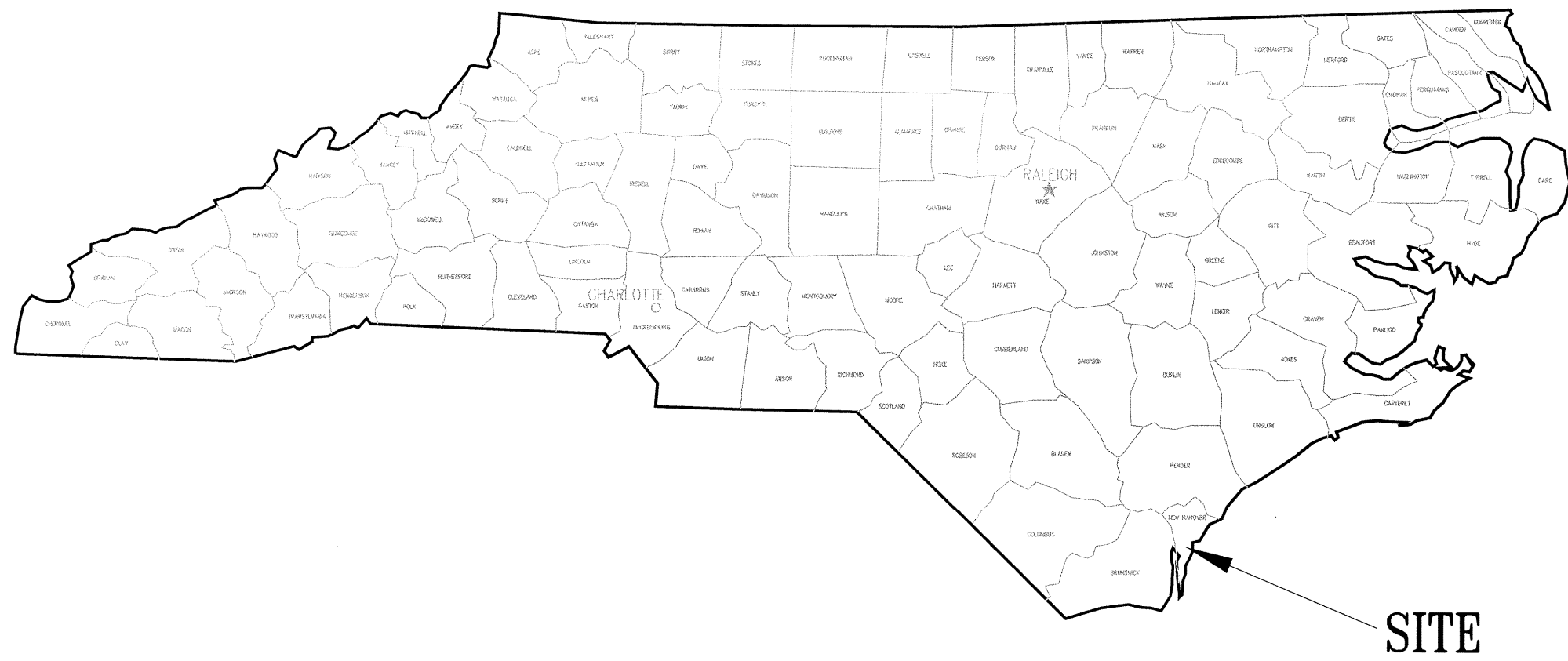
AutoZone Store No. 6486

3913 Oleander Drive

(US Hwy 76)

Wilmington, NC

New Hanover County



SITE

Applicant / Future Property Owner:

AutoZone Development, LLC

c/o: Wade Davis

123 S. Front Street, 3rd Floor

Memphis, Tennessee 38103

(901) 495-8701

wade.davis@autozone.com

Architect:

AutoZone Development, LLC

c/o: George Callow

123 S. Front Street, 3rd Floor

Memphis, Tennessee 38103

(901) 495-8705

Plan Submittal Date:

September 29, 2015

EXISTING PROPERTY OWNERS:

INDEPENDENCE GENE LLC

HENRY E. MILLER, JR

1713 N. LUMINA AVENUE

WRIGHTSVILLE BEACH, NC 28480

910-256-2778

CITY OF WILMINGTON NOTES:

1. Prior to any clearing, grading or construction activity, tree protection fencing shall be installed around protected trees or groves of trees. No construction workers, tools, materials, or vehicles are permitted within the tree protection fencing.
2. Any trees and/or areas designated to be protected must be properly barricaded with fencing and protected throughout construction to ensure that no clearing, grading or staging of materials will occur in those areas.
3. No equipment is allowed on site until all tree protection fencing and silt fencing is installed and approved. Protective fencing is to be maintained throughout the duration of the project, and contractors shall receive adequate instruction on tree protection methods.
4. All pavement markings in public rights-of-way and for driveways are to be thermoplastic and meet City and/or NCDOT standards.
5. Once streets are open to traffic, contact Traffic Engineering regarding the installation of traffic and street name signs. Proposed street names must be approved prior to installation of street name signs.
6. Traffic control devices (including signs and pavement markings) in areas open to public traffic are to meet MUTCD (Manual on Uniform Traffic Control Devices) standards.
7. Contact Traffic Engineering at 910-341-7888 to ensure that all traffic signal facilities and equipment are shown on the plan.
8. Call Traffic Engineering at 910-341-7888 forty-eight (48) hours prior to any excavation in the right-of-way.
9. Traffic Engineering must approve of pavement marking prior to actual striping.
10. All parking stall markings and lane arrows within the parking areas shall be white.
11. All traffic control signs and markings off the right-of-way are to be maintained by the property owner.
12. Stop signs and street signs to remain in place during construction.
13. Tactile warning mats will be installed on all wheelchair ramps.
14. A utility cut permit is required for each open cut of a city street.
15. Any broken or missing sidewalk panels will be replaced.
16. Contact Karen Dixon at 910-341-7888 to discuss street lighting options.
17. Water and sewer service shall meet Cape Fear Public Utility Authority (CPFA) details and specifications.
18. Project shall comply with CPFA Cross Connection Control requirements. Water meter(s) cannot be released until all requirements are met and the State has given their final approval. Call 910-343-3910 for information.
19. If the contractor desires CPFA water for construction, he shall apply in advance for this service and must provide a reduced pressure zone (RPZ) backflow prevention device on the developer's side of the water meter box.
20. Any irrigation system supplied by CPFA water shall comply with the CPFA Cross Connection Control regulations. Call 919-343-3910 for information.
21. Any irrigation system shall be equipped with a rain and freeze sensor.
22. Any backflow prevention devices required by the CPFA will need to be on the list of approved devices by USFCCOHR or ASSE.
23. Contractor to field verify existing water and sewer service locations, sizes and materials prior to construction. Engineer to be notified of any conflicts.
24. Contractor shall maintain all-weather access for emergency vehicles at all times during construction.
25. Underground fire line(s) must be permitted and inspected by the Wilmington Fire Department from the public right-of-way to the building. Contact the Wilmington Fire Department Division of Fire and Life Safety at 910-341-0696.
26. No obstructions are permitted in the space between thirty (30) inches and ten (10) feet above the ground within the triangular sight distance.
27. Contact the North Carolina One Call Center at 1-800-632-4949 prior to doing any digging, clearing, or grading.

Civil Engineering Plans Prepared by:

Triangle Site Design, PLLC

c/o: Matt Lowder, PE

4006 Barrett Drive, Suite 203

Raleigh, NC 27609

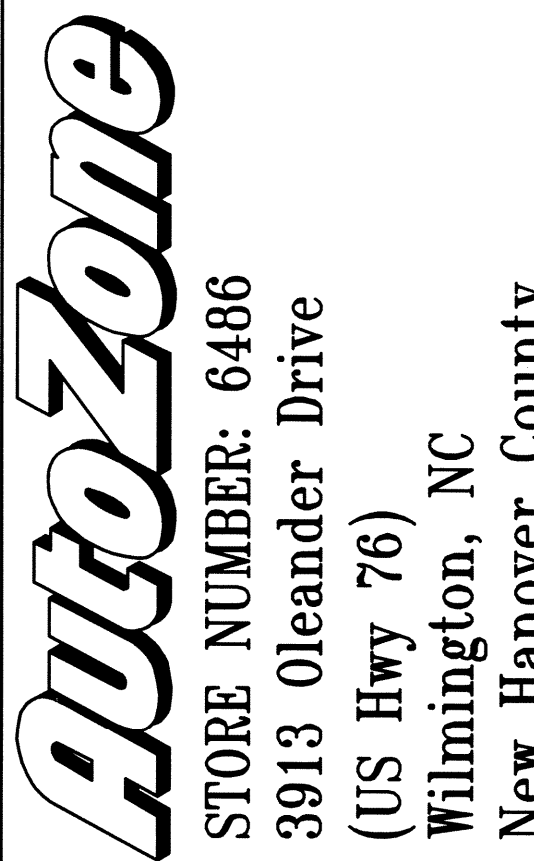
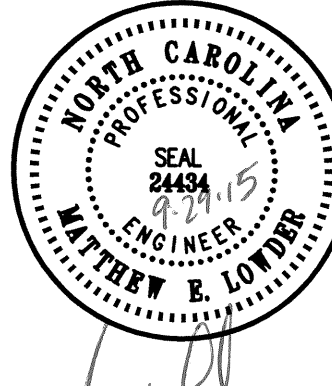
919-553-6570

mlowder@trianglesitedesign.com



CONSULTANT:
TRIANGLE SITE DESIGN, PLLC
4006 BARRETT DR, STE 203
RALEIGH, NC 27609
(919) 553-6570
LICENSE #P-0619

SEAL:



OWNER/DEVELOPER:
AUTOZONE DEVELOPMENT, LLC
ATTN. WADE DAVIS
123 S. FRONT STREET
3RD FLOOR
MEMPHIS, TN 38103-2107
PHONE: (901) 495-8701
FAX: (901) 495-8969
wade.davis@autozone.com
For Bidding & Contractor Information Contact:
F.W. Dodge Plan Room Tel: (615) 884-1017

REVISIONS:

LAYOUT COORD: MEL

PLANNING MGR. MEL

DRAWING BY: MEL

DATE: SEPTEMBER 29, 2015

JOB NUMBER: 010048

TITLE:

COVER SHEET

SHEET NUMBER:

C0.0

COMMENTS:

INDEX OF DRAWINGS

C0.0	COVER SHEET
C1.0	SITE PLAN
C1.A	SITE DETAILS
C1.1	GRADING & DRAINAGE PLAN
C1.2	UTILITY PLAN
C1.3	EROSION CONTROL PLAN
C1.4	CONSTRUCTION DETAILS
C1.5	STORMWATER MANAGEMENT DETAILS
L1.0	LANDSCAPE PLAN
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LP1.0	LIGHTING PLAN
1 of 2	ALTA/ACSM LAND TITLE SURVEY
2 of 2	ALTA/ACSM LAND TITLE SURVEY



Know what's below.
Call before you dig.



TRAFFIC CONTROL NOTES:

THE DISTRICT OFFICE OF THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION (NCDOT) MUST BE NOTIFIED AT LEAST 24 HOURS PRIOR TO CONSTRUCTION.

TRAFFIC MUST BE MAINTAINED AT ALL TIMES AND TRAFFIC CONTROL MUST COMPLY WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND WITH THE CURRENT EDITION OF THE NCDOT STANDARDS AND SPECIFICATIONS.

ALL LANES OF TRAFFIC ARE TO BE OPEN DURING THE HOURS OF 6:00 AM TO 9:00 AM AND FROM 4:00 PM TO 6:00 PM. A MINIMUM OF ONE 12-FOOT LANE SHALL BE MAINTAINED AT ALL TIMES.

ALL ROADWAY SIGNS WHICH ARE TO BE REMOVED DURING CONSTRUCTION ARE TO BE REINSTALLED AS SOON AS POSSIBLE.

DURING NON-WORKING HOURS, EQUIPMENT IS TO BE PARKED AS CLOSE TO THE RIGHT OF WAY LINE AS POSSIBLE AND BE PROPERLY BARRICADED IN ORDER TO PREVENT ANY EQUIPMENT OBSTRUCTION WITHIN THE TRAVEL LANE.

ALL CONTRACTORS DOING WORK WITHIN STATE RIGHT OF WAY ARE TO HAVE A COPY OF THESE PLANS ON THE JOB SITE.

WHEN PERSONNEL AND/OR EQUIPMENT ARE WITHIN 5' OF AN OPEN TRAVEL LANE, THE CONTRACTOR SHALL CLOSE THE OPEN TRAVEL LANE ADJACENT TO THE WORK AREA USING APPROPRIATE TRAFFIC CONTROL MEANS AND METHODS UNLESS WORK IN THAT AREA IS PROTECTED BY BARRIER OR GUARDRAIL.

THE CONTRACTOR SHALL NOT WORK SIMULTANEOUSLY ON BOTH SIDES OF AN OPEN TRAVEL WAY WITHIN THE SAME LOCATION ON A TWO-LANE, TWO-WAY ROAD.

THE MAXIMUM LENGTH OF A LANE CLOSURE ON THE PROJECT SHALL BE ONE MILE, MEASURED FROM THE BEGINNING OF THE MERGE TAPER TO THE END OF THE LANE CLOSURE.

THE CONTRACTOR SHALL BACKFILL AT 6:1 SLOPE UP TO THE EDGE AND ELEVATION OF EXISTING PAVEMENT IN AREAS ADJACENT TO AN OPENED TRAVEL LANE THAT HAS A DROP-OFF OF MORE THAN 3".

WHEN BACKFILL IS REQUIRED, IT SHALL BE AT NO EXPENSE TO THE OWNER.

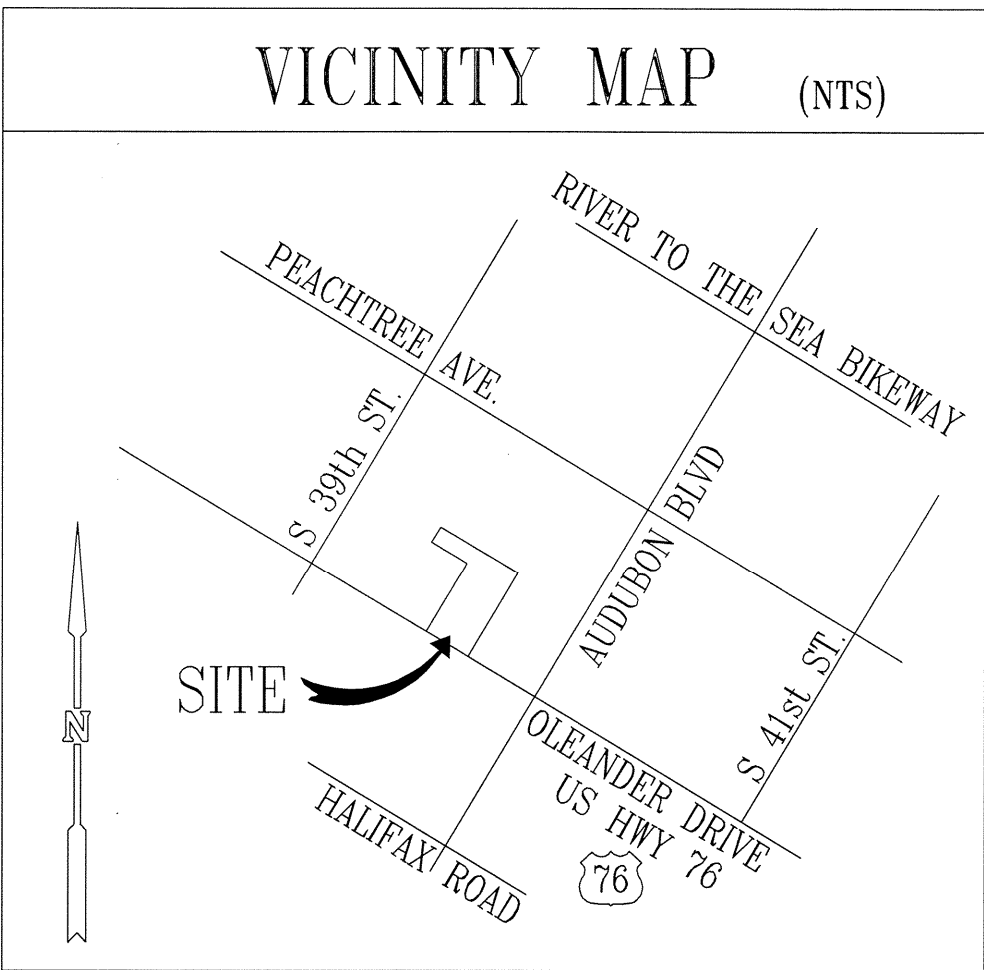
THE MAXIMUM DIFFERENCE IN ELEVATION BETWEEN OPEN LANES OF TRAFFIC SHALL BE 2".

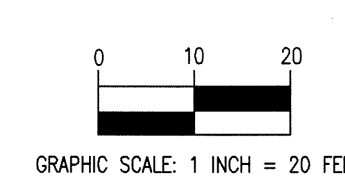
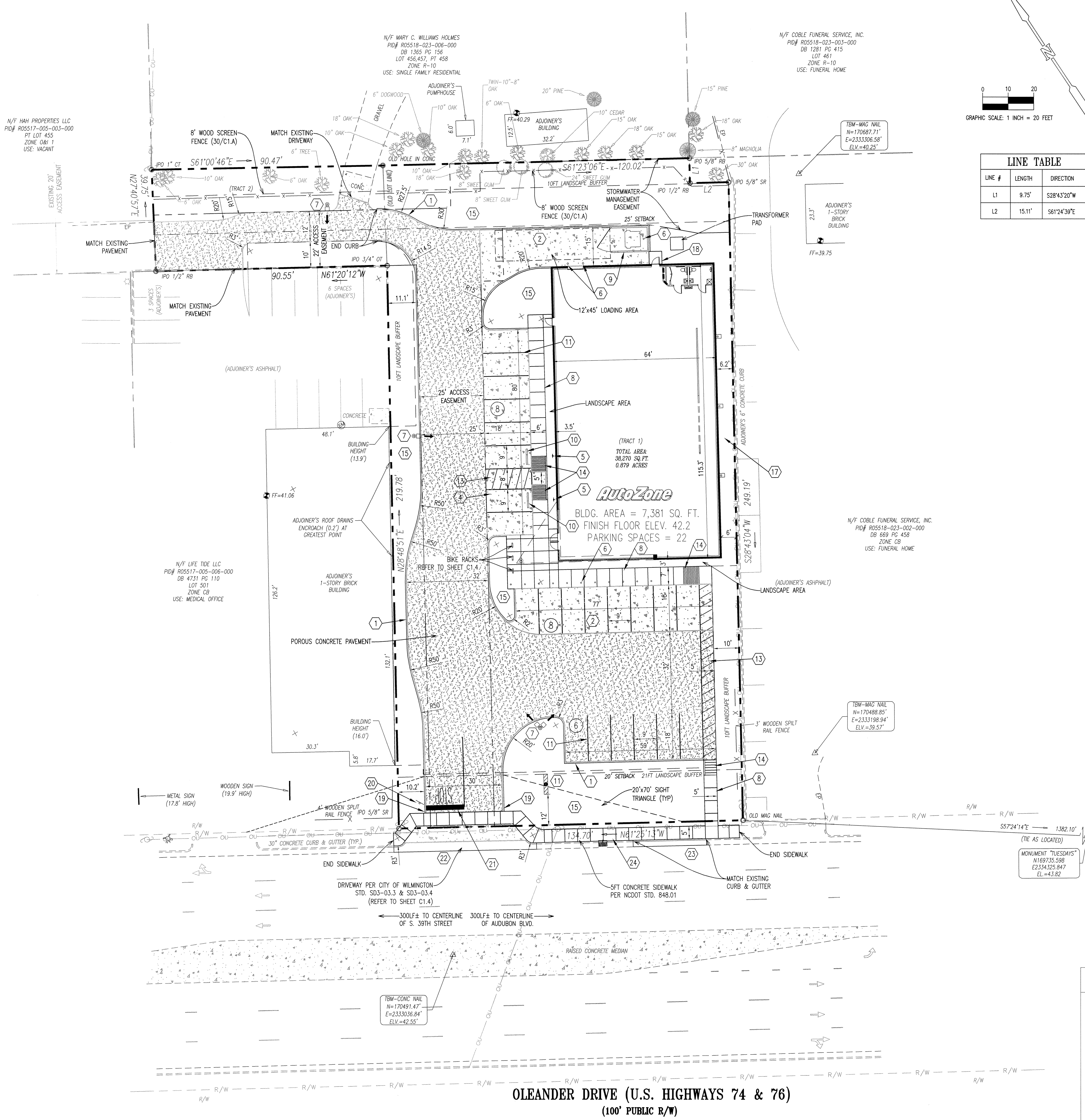
WHEN LANE CLOSURES ARE NOT IN EFFECT, CHANNELING DEVICES IN WORK AREAS SHALL BE SPACED NO GREATER THAN TWICE THE POSTED SPEED LIMIT, EXCEPT 10' ON-CENTER IN RADI, AND SHALL BE SET 3' OFF THE EDGE OF AN OPEN TRAVEL-WAY.

DURING INSTALLATION, PROPER TRAFFIC CONTROL DEVICES, SIGNS, ETC. BE INSTALLED TO ENSURE PUBLIC SAFETY IN ACCORDANCE WITH THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION'S STANDARDS AND SPECIFICATIONS.

ALL DISTURBED AREAS ARE TO BE FULLY RESTORED TO NCDOT MINIMUM ROADWAY STANDARDS.

MATERIALS AND WORKMANSHIP SHALL CONFORM TO THE NCDOT ROADWAY STANDARDS AND SPECIFICATIONS (LATEST EDITION).





LINE TABLE		
LINE #	LENGTH	DIRECTION
L1	9.75'	S28°43'20\"W
L2	15.11'	S61°24'39\"E

IMPERVIOUS AREA SUMMARY	
TOTAL SITE AREA =	38,270 SF (0.88 AC)
EXISTING IMPERVIOUS AREA =	20,153 SF (0.46 AC TO BE DEMOLISHED)
PROPOSED BUILDING =	7,381 SF
PROPOSED ON-SITE SIDEWALK =	1,523 SF
PROPOSED PERVIOUS CONCRETE =	2,984 SF (11,937 SF TOTAL, 75% PERVIOUS, 25% BUA (TYPE B 50LS))
PROPOSED ON-SITE PAVEMENT =	4,188 SF
TOTAL ON-SITE IMPERVIOUS AREA =	16,076 SF (0.37 AC) PERCENTAGE = 42.0%
TOTAL ON-SITE OPEN SPACE =	22,194 SF (0.51 AC) PERCENTAGE = 58.0%
NET DECREASE IN IMPERVIOUS AREA =	4,077 SF (0.09 AC) (EXEMPT FROM 25-YEAR PRE-/POST ATTENUATION REQUIREMENT)

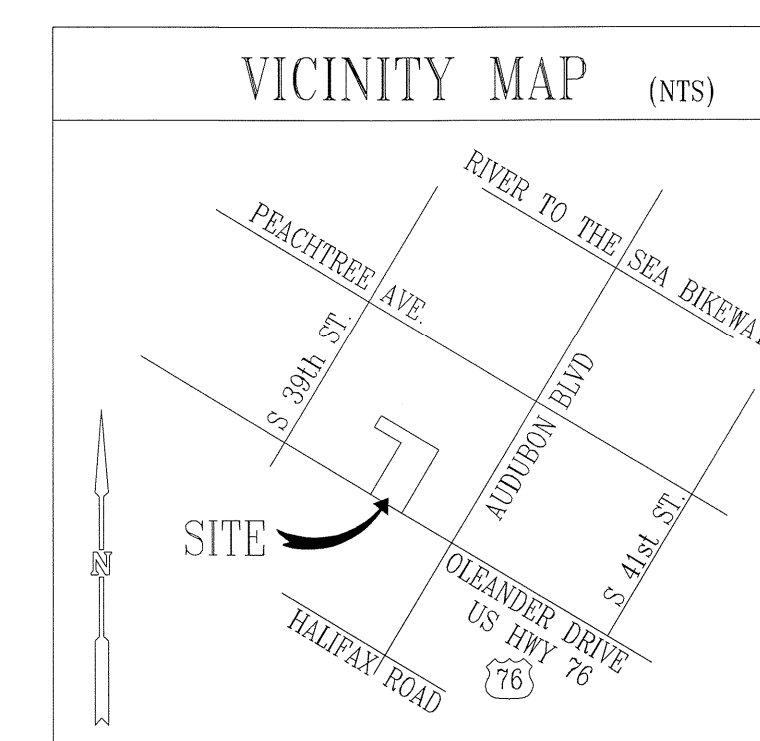
- ### GENERAL NOTES
- Property line and right-of-way monuments shall not be disturbed by construction. If disturbed, they shall be reset to their original locations at the Contractor's expense by a Registered Land Surveyor.
 - Proof Rail Building and all parking areas. Notify AutoZone Inc. of any unacceptable areas.
 - Building dimensions shown on the Civil Engineering Plans are for reference purposes only. Contractor shall use the Architectural and Structural Plans for exact building dimensions.
 - All site dimensions are referenced to the face of curbs or edge of paving unless otherwise noted.
 - All sidewalks, curb and gutter, street paving, curb cuts, driveway approaches, handicap ramps, etc. constructed outside the property line in the right-of-way shall conform to all municipal and/or State specifications and requirements.
 - All disturbance incurred to any adjoining property due to construction or demolition shall be restored to the previous condition or better, and to the satisfaction of the City or State Authority.
 - The Contractor shall provide As-Built records of all construction (including underground utilities) to AutoZone Inc. at the end of construction.

PAVEMENT LEGEND	
	ASPHALT PAVING - SEE DETAIL 5/C1.A (ALTERNATE BID) CONCRETE PAVING - SEE DETAIL 4/C1.A
	CONCRETE PAVING - SEE DETAIL 4/C1.A
	POROUS CONCRETE PAVING - SEE DETAIL SHEET C1.5

SIGN LEGEND	
LEGEND	QTY.
	2
	3

BULK AREA REQUIREMENTS		
LOCATION: 3913 OLEANDER DRIVE (US HWY 76)		
ZONE: CB (COMMUNITY BUSINESS)		
USE: RETAIL AUTO PARTS SALES		
PIN ID: ROSS518-023-001-000		
ITEM	REQUIREMENTS	PROVIDED
MINIMUM LOT AREA	0.5 AC	0.879 AC
MINIMUM LOT FRONTAGE	80FT	135FT
MINIMUM FRONT SETBACK	20FT	102FT
MINIMUM SIDE SETBACK	0FT	6FT
MINIMUM REAR SETBACK	25FT (ADJACENT RESIDENTIAL)	25FT
MAXIMUM BUILDING HEIGHT	45 FT (3 STORIES)	21FT± (1 STORY)
NUMBER OF STORIES		
SIGN SETBACK	10 FT	12 FT
LOT COVERAGE (BUILDING)	30%	19.3%
STREET YARD BUFFER	20FT STREET YARD	20FT STREET YARD
PERIMETER PARKING BUFFER	10FT PARKING BUFFER	10FT PARKING BUFFER
FOUNDATION PLANTINGS	12% OF BUILDING FACE AREA	12% OF BUILDING FACE AREA
ZONING BUFFER	10FT ADJACENT RESIDENTIAL WITH FENCE	10FT ADJACENT RESIDENTIAL WITH FENCE
CAMA LAND USE CLASSIFICATION: DEVELOPED		
REFERENCE IS MADE TO THE CITY OF WILMINGTON ZONING ORDINANCE		

PARKING INFORMATION		
ITEM	REQUIREMENTS	PROVIDED
BUILDING SIZE	N/A	7,381 SF
PARKING REQUIRED	MIN SPACES = 18 (1 SPACE/400 SF GFA) MAX SPACES = 37 (1 SPACE/200 SF GFA)	22 SPACES
MIN. PARKING DIM.	8.5 FT x 18 FT	9 FT x 18 FT
MIN. DRIVEWAY WIDTH	24 FT	30 FT
HANDICAP SPACES	2 SPACE	2 SPACES
LOADING SPACE	1 REQUIRED (12'x45')	1 SPACE
BICYCLE SPACES	5 SPACES	5 SPACES



KEYNOTES

- Building & Paving**
 - Concrete curb - see details 1 & 2/ C1.A
 - Concrete paving - see detail 4/C1.A. Expansion and Control joints - see details 21 & 22/C1.A. Maximum spacing for Control joints is 15'-0" o.c. each way.
 - Asphalt paving - see detail 5/C1.A. G.C. to provide alternate bid for concrete paving
 - Handicap parking area - see details 6 & 7/C1.A
 - Handicap parking sign - see detail 12/C1.A
 - Pipe guard - see details 14, 15 & 16/C1.A
 - Concrete light pole base - see detail 13/C1.A. Locate 3'-0" from back of curb or sidewalk to center of base. Aim light fixture in direction as indicated.
 - Concrete sidewalk - see detail 19 & 20/C1.A for sidewalks around building.
 - Dumpster layout - 6' wood enclosure w/ metal or composite PVC gate - see details 8, 9, 10 & 11/C1.A
 - 6'-0" long concrete wheel stop, pinned to pavement - see detail 17/C1.A. Locate 2'-6" from face of curb or sidewalk.
 - Pylon Sign 5' x 80' x 25' hgt. See signage drawings (Under separate permit)
- 4" wide parking stripe painted white (typ.)
 - 4" wide diagonal stripes painted white @ 2 ft. o.c.
 - Concrete handicap ramp - maximum slope 1:12 (8.33%) (per A.D.A. requirements). Maximum cross slope 1:50 (2.00%)
 - New landscape area - provide sod and 3" topsoil - see sheet L1.0 for additional information
 - Slope grade from back of curb down to match the existing grade @ property line.
 - Connect building down spouts to storm drainage system
 - 5' x 5' concrete pad
 - Slope end of curb 4:1
 - Stop Sign - see detail 26/C1.A
 - Pavement markings - see detail 25 & 26/C1.A
 - New curb cut & approach per local codes & specs.
 - New concrete curb & gutter to meet all local & state D.O.T. requirements
 - New sidewalk to meet all local & state D.O.T. requirements
 - Remove existing curb, gutter, pavement, sidewalk and back fill per local & state D.O.T. requirements

TRIANGLE
SITE DESIGN

CONSULTANT:
TRIANGLE SITE DESIGN, PLLC
4006 BARRETT DR, STE 203
RALEIGH, NC 27609
(919) 553-6570
LICENSE #P-0619

SEAL:

AutoZone

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For Bidding & Contractor Information Contact:
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REVISIONS:

LAYOUT COORD: MEL
PLANNING MGR. MEL
DRAWING BY: MEL
DATE: SEPTEMBER 29, 2015
JOB NUMBER: 010048
TITLE:

SITE PLAN

SHEET NUMBER:

C1.0

COMMENTS:

N/F LHA PROPERTIES LLC
PID# 160517-003-003-000
PT LOT 455
ZONE OH-1
USE: VACANT

N/F LIFE TIDE LLC
PID# 160517-003-003-000
DB 4731 PG 110
LOT 501
ZONE OH-1
USE: MEDICAL OFFICE

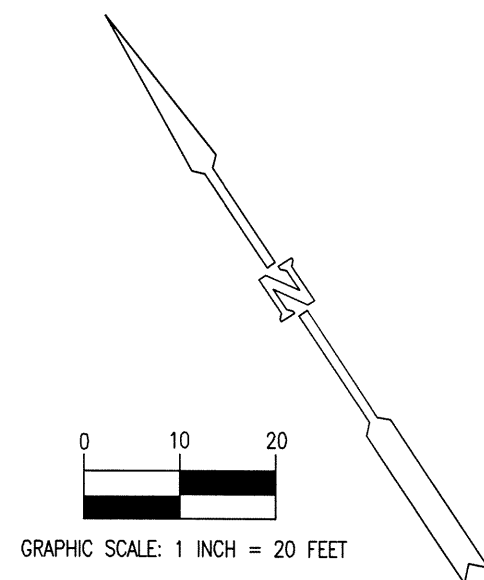
N/F MARY C. WILLIAMS HOLMES
PID# 160518-023-006-000
DB 1365 PG 126
LOT 456, 457, PT 458
ZONE R-10
USE: SINGLE FAMILY RESIDENTIAL

N/F COBLE FUNERAL SERVICE, INC.
PID# 160518-023-003-000
DB 1281 PG 415
LOT 461
ZONE R-10
USE: FUNERAL HOME

N/F COBLE FUNERAL SERVICE, INC.
PID# 160518-023-003-000
DB 668 PG 458
ZONE OH-1
USE: FUNERAL HOME

LEGEND:

- TS.....TOP OF SIDEWALK
- TC.....TOP OF CURB
- GT.....GUTTER FLOW LINE
- TP.....TOP OF PAVEMENT
- TW.....TOP OF WALL
- BW.....BOTTOM OF WALL
- CB.....CATCH BASIN
- GI.....GRATE INLET
- FL.....FLARED END SECTION
- YI.....YARD INLET
- GR.....GROUND
-FLOW DIRECTION ARROW



FLOOD NOTE:

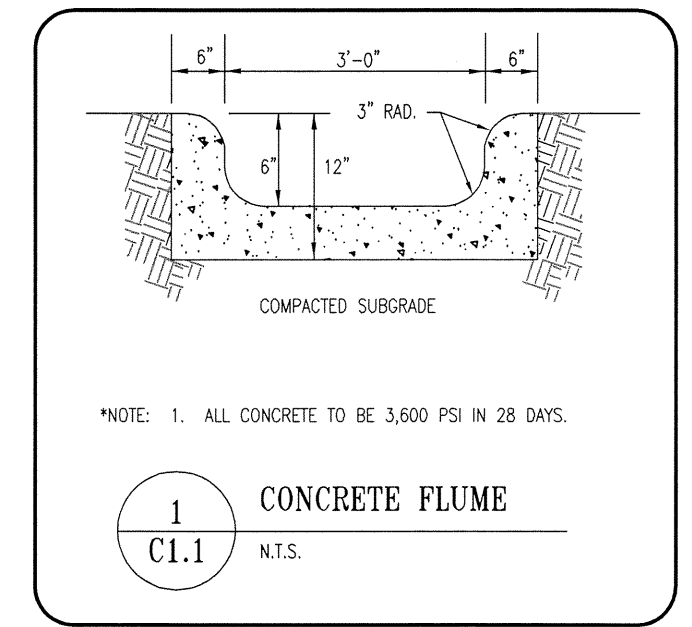
PER REVIEW OF FEMA FIRM PANEL NO. 3700317000
DATED 4/3/2006 THIS PARCEL IS NOT LOCATED
WITHIN A SPECIAL FLOOD HAZARD AREA. THE FLOOD
ZONE DESIGNATION IS ZONE X.
(FLOOD ZONES SUBJECT TO CHANGE BY FEMA)

WETLAND NOTE:

THERE ARE NO WETLANDS AND/OR STREAM
FEATURES ON THE PROPOSED PARCEL

GRADING NOTES:

1. REFER TO THE SITE PLAN FOR RELATED NOTES.
2. ALL CONTOURS AND SPOT ELEVATIONS REFLECT FINISHED GRADES.
3. ALL ELEVATIONS ARE IN REFERENCE TO THE BENCHMARK, AND THIS MUST BE VERIFIED BY THE GENERAL CONTRACTOR PRIOR TO GROUND BREAKING.
4. THE CONTRACTOR SHALL IMMEDIATELY REPORT TO OWNER ANY DISCREPANCIES FOUND BETWEEN ACTUAL FIELD CONDITIONS AND CONSTRUCTION DOCUMENTS AND SHALL WAIT FOR INSTRUCTION PRIOR TO PROCEEDING.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND PROTECTING EXISTING UTILITIES, AND SHALL REPAIR ALL DAMAGE TO EXISTING UTILITIES THAT OCCUR DURING CONSTRUCTION.
6. THE CONTRACTOR SHALL BLEND NEW EARTHWORK SMOOTHLY TO TRANSITION BACK TO EXISTING GRADE.
7. LIMITS OF CLEARING SHOWN ON GRADING PLAN ARE BASED UPON THE APPROXIMATE CUT AND FILL SLOPE LIMITS, OR OTHER GRADING REQUIREMENTS.
8. THE PROPOSED CONTOURS SHOWN IN DRIVES AND PARKING LOTS AND SIDEWALKS ARE FINISHED ELEVATIONS INCLUDING ASPHALT. REFER TO PAVEMENT CROSS SECTION DATA TO ESTABLISH CORRECT SUB-BASE OR AGGREGATE BASE COURSE ELEVATIONS TO BE COMPLETED UNDER THIS CONTRACT.
9. THE CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE SO THAT RUNOFF WILL DRAIN BY GRAVITY FLOW ACROSS NEW PAVEMENT AREAS TO NEW OR EXISTING DRAINAGE INLETS OR SHEET OVERLAND.
10. ANY GRADING, BEYOND THE LIMITS OF CONSTRUCTION AS SHOWN ON THE GRADING PLAN, IS SUBJECT TO A FINE.
11. GRADING MORE THAN ONE ACRE WITHOUT AN APPROVED EROSION CONTROL PLAN IS SUBJECT TO A FINE.
12. STABILIZATION IS THE BEST FORM OF EROSION CONTROL. TEMPORARY SEEDING IS NECESSARY TO ACHIEVE EROSION CONTROL ON DENUDATED AREAS AND ESPECIALLY WHEN THE CONSTRUCTION SEQUENCE REQUIRES IT.
13. ALL DISTURBED AREAS ARE TO BE SEEDED OR LANDSCAPED WITHIN 14 DAYS OF COMPLETION OF GRADING.
14. EXISTING GRADES, CONTOURS, UTILITIES AND OTHER EXISTING FEATURES FROM FIELD RUN SURVEY.
15. THE CONTRACTOR SHALL INCLUDE IN THE CONTRACT PRICE ANY DE-WATERING NECESSARY TO CONSTRUCT THE PROJECT AS SHOWN ON THE PLANS.
16. THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN AND IMPLEMENTATION OF ALL SHEETING, SHORING, BRACING AND SPECIAL EXCAVATION MEASURES REQUIRED TO MEET OSHA, FEDERAL, STATE, AND LOCAL REGULATIONS PURSUANT TO THE INSTALLATION OF THE WORK INDICATED ON THESE DRAWINGS. THE DESIGN ENGINEER ACCEPTS NO RESPONSIBILITY FOR THE DESIGN(S) TO INSTALL SAID ITEMS.
17. THE CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATION, ELEVATION, AND DIMENSIONS OF EXIT DOORS, RAMPS, BUILDING DIMENSIONS, AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
18. ALL FILL MATERIALS, EXISTING BUILDING FOUNDATIONS, PAVEMENT AND UTILITY STRUCTURES, TOPSOIL, AND ANY OTHER DELETERIOUS MATERIALS SHALL BE COMPLETELY REMOVED FROM WITHIN THE BEARING ZONE BELOW THE STRUCTURE.
19. ALL FOUNDATION EXCAVATION SHALL BE INSPECTED BY A QUALIFIED GEOTECHNICAL REPRESENTATIVE TO DETERMINE WHETHER UNSUITABLE MATERIAL MUST BE REMOVED. ALL UNSUITABLE MATERIAL SHALL BE REMOVED, BACKFILLED AND COMPACTED AS REQUIRED BY THE GEOTECHNICAL REPRESENTATIVE.
20. ALL CUT OR FILL SLOPES SHALL BE 3:1 OR FLATTER UNLESS OTHERWISE NOTED OR DEPICTED.
21. THE CONTRACTOR SHALL ADHERE TO ALL TERMS & CONDITIONS AS OUTLINED IN THE GENERAL N.P.D.E.S PERMIT FOR STORM WATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
22. CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
23. CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
24. ALL UN-SURFACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL. CONTRACTOR SHALL APPLY STABILIZATION FABRIC TO ALL SLOPES 3H:1V OR STEEPER.
25. CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES AND BE CONSTRUCTED TO SAME.



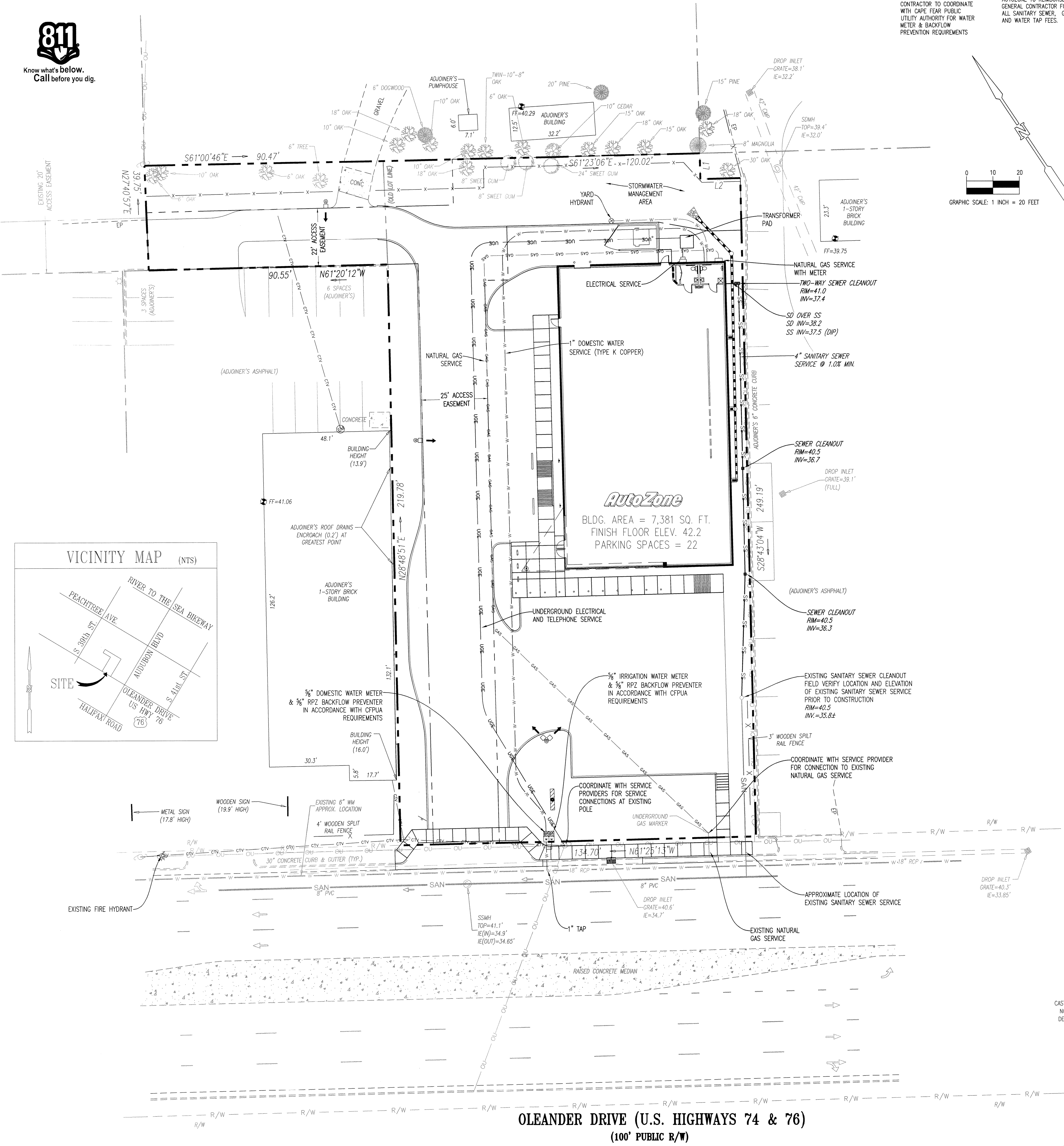
DRAINAGE NOTES:

1. A MINIMUM GRADE OF 0.50 % SHALL BE MAINTAINED ON ALL PIPES, UNLESS OTHERWISE NOTED.
2. PIPE LENGTHS AND SLOPES INDICATED ON THE PLANS ARE APPROXIMATE ONLY.
3. UNDERGROUND UTILITY LINES SHALL BE INSTALLED IN ACCORDANCE WITH THE FOLLOWING STANDARDS IN ADDITION TO OTHER APPLICABLE CRITERIA:
 - A. NO MORE THAN 500 LF OF TRENCH MAY BE OPENED AT ONE TIME.
 - B. EXCAVATED MATERIAL SHALL BE PLACED ON THE UPHILL SIDE OF TRENCHES.
 - C. EFFLUENT FROM DE-WATERING OPERATIONS SHALL BE FILTERED OR PASSED THROUGH AN APPROVED SEDIMENT TRAPPING DEVICE, OR BOTH, AND DISCHARGED IN A MANNER THAT DOES NOT ADVERSELY AFFECT FLOWING STREAMS OR OFF-SITE PROPERTY.
 - D. MATERIAL USED FOR BACK-FILLING TRENCHES SHALL BE PROPERLY COMPACTED IN ORDER TO MINIMIZE EROSION AND PROMOTE STABILIZATION.
 - E. RE-STABILIZATION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH THE EROSION AND SEDIMENT CONTROL REGULATIONS.
 - F. APPLICABLE SAFETY REGULATIONS SHALL BE COMPLIED WITH.
4. CATCH BASINS, MANHOLES, FRAMES, GRATES, ETC. SHALL MEET THE REQUIREMENTS OF THE LATEST EDITION OF THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION STANDARD DRAWINGS. REFERENCE THE FOLLOWING STANDARD DETAILS:
 - 840.02 - CONCRETE CATCH BASIN
 - 840.03 - FRAME, GRATES, AND HOOD FOR CATCH BASINS
 - 840.04 - CONCRETE OPEN THROAT CATCH BASIN
 - 840.14 - CONCRETE DROP INLET
 - 840.16 - DROP INLET FRAME AND GRATES
 - 840.31 - CONCRETE JUNCTION BOX
 - 840.35 - TRAFFIC BEARING GRATED DROP INLET
 - 840.45 - PRECAST DRAINAGE STRUCTURE
 - 840.52 - PRECAST MANHOLE
 - 838.80 - PRECAST CONCRETE ENDWALL
5. ALL PIPES SHALL BE LAID ON STRAIGHT ALIGNMENTS AND EVEN GRADES USING A PIPE LASER OR OTHER ACCURATE METHOD.
6. STORM PIPE SHALL BE AS FOLLOW UNLESS OTHERWISE NOTED:
 - TYPE 1: RCP, CLASS III PER ASTM C-76, WITH FLEXIBLE PLASTIC BITUMEN GASKETS AT JOINTS.
 - TYPE 2: HIGH DENSITY POLYETHYLENE PIPE (HDPE) - AASHTO DESIGNATION M252 TYPE S, M294 TYPE S AND MP7-97 TYPE S, SMOOTH INTERIOR/ANNUAL EXTERIOR. ONLY PERMITTED WHEN SPECIFICALLY INDICATED ON THE CONSTRUCTION DRAWINGS. PIPE SHALL BE INSTALLED IN ACCORDANCE WITH PIPE MANUFACTURER'S INSTALLATION GUIDELINES. PIPE JOINTS AND FITTINGS SHALL BE WATERTIGHT.
7. ALL STORM DRAINAGE WITHIN THE PUBLIC ROADS SHALL BE CLASS III REINFORCED CONCRETE PIPE UNLESS OTHERWISE NOTED.
8. EXISTING DRAINAGE STRUCTURES TO BE INSPECTED AND REPAIRED AS NEEDED, AND EXISTING PIPES TO BE CLEANED OUT TO REMOVE SILT AND DEBRIS.
9. IF ANY EXISTING STRUCTURES TO REMAIN ARE DAMAGED DURING CONSTRUCTION IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER.
10. ALL STORM PIPE ENTERING STRUCTURES SHALL BE GROUDED TO ASSURE CONNECTION AT STRUCTURE IS WATERTIGHT.
11. PRECAST STRUCTURES MAYBE USED AT CONTRACTORS OPTION.
12. ALL STORM SEWER MANHOLES IN PAVED AREAS SHALL BE FLUSH WITH PAVEMENT, AND SHALL HAVE TRAFFIC BEARING RING & COVERS. MANHOLES IN UNPAVED AREAS SHALL BE 6" ABOVE FINISH GRADE. LIDS SHALL BE LABELED "STORM SEWER".
13. STRUCTURE TOP ELEVATIONS SHOWN HERE ARE APPROXIMATE. CONTRACTOR SHALL ADJUST AS NECESSARY.
14. R/W ELEVATIONS AS NOTED ARE TO THE GUTTER FLOW LINE.

OLEANDER DRIVE (U.S. HIGHWAYS 74 & 76)
(100' PUBLIC R/W)



Know what's below.
Call before you dig.



NOTE:
CONTRACTOR TO COORDINATE
WITH CAPE FEAR PUBLIC
UTILITY AUTHORITY FOR WATER
METER & BACKFLOW
PREVENTION REQUIREMENTS

NOTE:
AUTOZONE TO REIMBURSE
GENERAL CONTRACTOR FOR
ALL SANITARY SEWER, GAS,
AND WATER TAP FEES.

GENERAL UTILITY NOTES:

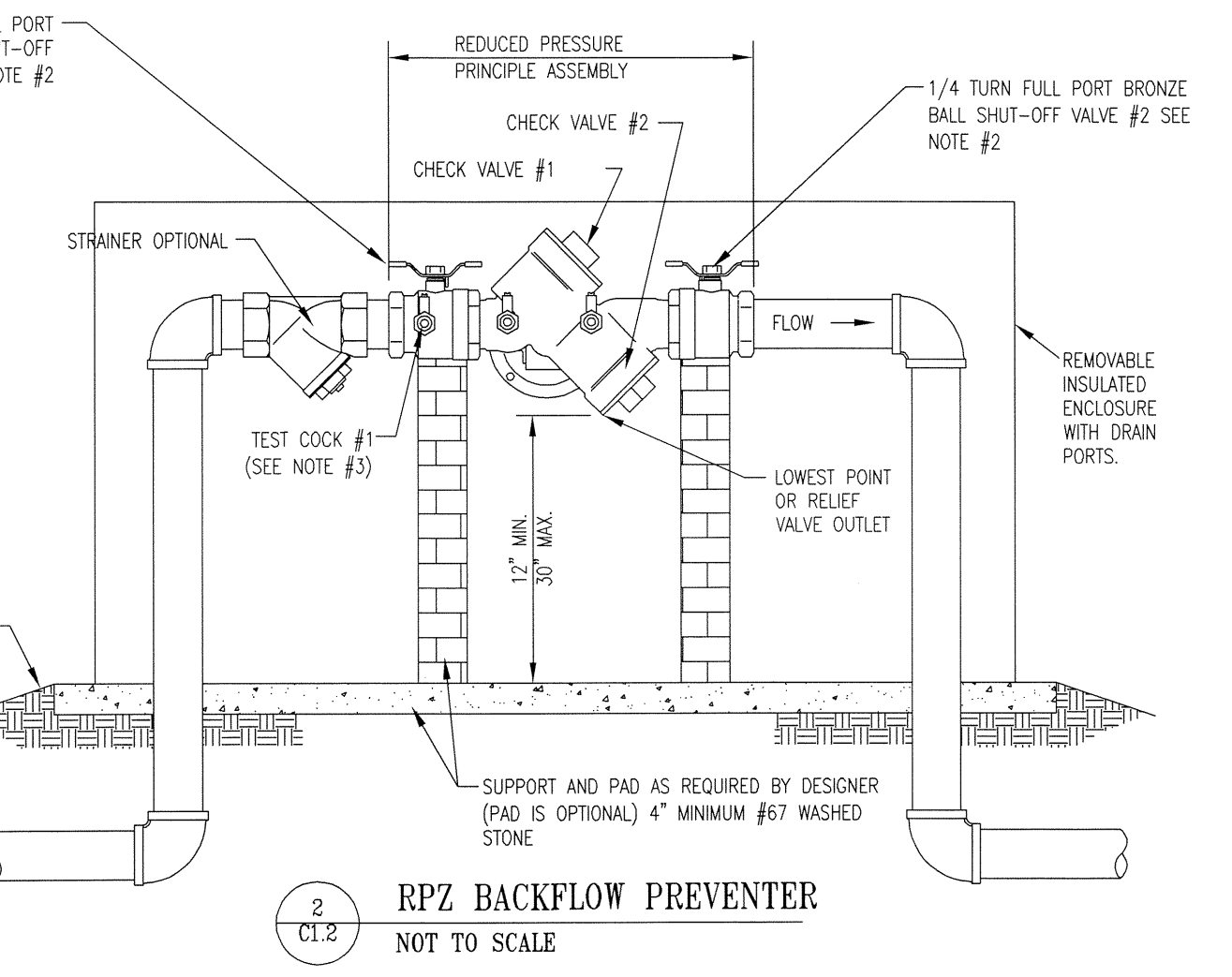
1. All utilities shown are approximate locations only and have been compiled from the latest available mapping. The exact location of all underground utilities shall be verified by the Contractor prior to the start of construction.
2. General Contractor to coordinate with the local Utility Companies for all locations and connections. A pre-construction meeting with the various Utility Companies, is required prior to the start of any construction activity.
3. The Contractor shall visit the site and verify the elevation and location of all utilities by various means prior to beginning any excavation. Test pits shall be dug at all locations where sewers cross existing utilities, and the horizontal and vertical locations of the utilities shall be determined. The Contractor shall contact AutoZone in the event of any unforeseen conflicts between existing and proposed utilities so that an appropriate modification may be made.
4. The Contractor shall insure that all Utility Companies and City Standards for materials and construction methods are met. The Contractor shall perform proper coordination with the respective Utility Company. The Contractor shall coordinate work to be performed by the various Utility Companies and shall pay all fees for connections, disconnection, relocation, inspections, and demolition. (AutoZone to reimburse General Contractor for all Sanitary Sewer and Water Tap Fees).
5. All valve boxes and curb boxes shall be adjusted to the final grade. All curb boxes shall be located in grassed areas unless indicated otherwise on the plans.
6. Sanitary lateral shall maintain (1" min. horizontal 1.5' vertical min.) separation distance from water lines unless otherwise shown, or additional protection measures will be required. Where water line crosses above sanitary lateral by less than 2' vertical, a concrete encasement shall be installed. Contractor shall center one joint of pipe at crossing.
7. This plan details pipe up to 5' from the building face. Refer to the building drawings for building connections. Supply and install pipe adapters as necessary.
8. All existing pavement where utility piping is to be installed shall be saw cut and replaced in accordance with the pavement repair requirements of the Governing Authority.
9. Copper pipe shall be type K tubing with compression fittings.
10. All sanitary sewer main lines shall be schedule 40 PVC pipe (except as noted on plans). All PVC pipe shall be installed in accordance with the manufacturer's recommended procedure.

UTILITY NOTES:

1. ALL WORK SHALL BE DONE IN STRICT ACCORDANCE WITH CAPE FEAR PUBLIC UTILITY AUTHORITY STANDARD DRAWINGS AND SPECIFICATIONS.
2. IN THE EVENT THAT A UTILITY ITEM IS NOT COVERED BY THESE PLANS, THEN THE STANDARDS AND SPECIFICATIONS CONTAINED IN THE CAPE FEAR PUBLIC UTILITY AUTHORITY REQUIREMENTS COVERING SUCH ITEMS SHALL APPLY.
3. WATER SERVICES 1-INCH SHALL BE TYPE "K" SOFT COPPER. ALL OTHER WATER MAINS, SERVICES, AND FITTINGS SHALL BE CEMENT-LINED DUCTILE IRON PIPE PER CAPE FEAR PUBLIC UTILITY AUTHORITY STANDARDS AND SPECIFICATIONS.
4. GROUND SURVEY SEWER MAINS SHALL BE DUCTILE IRON PIPE OR PVC PIPE AS SPECIFIED IN THESE PLANS AND CAPE FEAR PUBLIC UTILITY AUTHORITY STANDARDS AND SPECIFICATIONS. SANITARY SEWER SERVICES SHALL BE 4" SDR 35 PVC.
5. CLEAN-OUT SYMBOLS SHOWN ON THESE PLANS REPRESENT LOCATION OF SURFACE ACCESS POINT. CONTRACTOR SHALL LOCATE AND PROVIDE APPROPRIATELY BASED ON PIPE DEPTH.
6. ALL MANHOLES SHALL BE CONSTRUCTED IN ACCORDANCE WITH CAPE FEAR PUBLIC UTILITY AUTHORITY STANDARDS. MANHOLE DIAMETER SHALL VARY DEPENDING ON PIPE DIAMETER AND DEPTH, PER CAPE FEAR PUBLIC UTILITY AUTHORITY STANDARDS.
7. LOCATIONS AND SIZES OF EXISTING UTILITIES SHOWN ON THESE PLANS WERE TAKEN FROM MAPS PREPARED BY OTHERS. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR FIELD LOCATING ALL UTILITIES AND FOR DAMAGES RESULTING FROM FAILURE TO DO SO.
8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING RECORD DRAWINGS TO THE ENGINEER SHOWING THE LOCATION OF WATER AND SEWER SERVICES AND ANY LOCATIONS FROM PLANS MADE DURING CONSTRUCTION. THE BURNER CONTRACTOR SHALL PROVIDE THESE RECORD DRAWINGS TO THE OWNER.
9. WATER MAINS SHALL HAVE A MINIMUM COVER OF 36" BELOW PROPOSED GRADE.
10. ALL UNDERGROUND UTILITIES AND FIRE HYDRANTS MUST BE FUNCTIONALLY APPROVED PRIOR TO STRUCTURAL CONSTRUCTION.
11. THRUST BLOCKS SHALL BE PROVIDED AT ALL BENDS, TEES, AND FIRE HYDRANTS.
12. DIMENSIONS SHOWN ARE TO CENTRELINE OF PIPE OR FITTING.
13. ALL WATER AND SANITARY LEADS TO BUILDING SHALL END 3' OUTSIDE THE BUILDING LIMITS AS SHOWN ON PLAN AND SHALL BE PROVIDED WITH A TEMPORARY PLUG AT THE BUILDING FACE.
14. ALL TRENCHING, PIPE LAYING, AND BACKFILLING SHALL BE IN ACCORDANCE WITH GEORGIA REGULATIONS. MINIMUM TRENCH WIDTH SHALL BE 2 FEET.
15. GENERAL CONTRACTOR SHALL HAVE APPROVAL OF ALL COVERING AGENCIES HAVING JURISDICTION OVER THIS SYSTEM PRIOR TO INSTALLATION.
16. ALL FILL MATERIAL IS TO BE IN PLACE, AND COMPACTED BEFORE INSTALLATION OF PROPOSED UTILITIES.
17. CONTRACTOR SHALL NOTIFY THE WATER AUTHORITY INSPECTORS 72 HOURS BEFORE CONNECTING TO ANY EXISTING LINE.
18. ALL UTILITIES SHOULD BE KEPT TEN (10') AWAY (PARALLEL) OR WHEN CROSSING 24" VERTICAL CLEARANCE (OUTSIDE EDGE OF THE BUILDING EXCEEDS 80PSI).
19. PRESSURE REDUCING VALVES WILL BE REQUIRED ON THE DOMESTIC WATER MAINS IF THE STATIC PRESSURE AT THE BUILDING EXCEEDS 80PSI.
20. IN THE EVENT OF A VERTICAL CONFLICT BETWEEN WATERLINES, SANITARY LINES, CROWN LINES AND GAS LINES (EXISTING AND PROPOSED), THE SANITARY LINE SHALL BE LOCATED AT LEAST 18" FEET FROM WATER MAINS. THE MAXIMUM SHALL HAVE MEDICAL WASTE WITH APPROPRIATE THRUST BLOCKING AS REQUIRED TO PROVIDE A MINIMUM OF 24" CLEARANCE. MEETING REQUIREMENTS OF ANSI A21.10 OR ANSI 21.11 (MMA C-15) (CLASS 50).
21. DRAWINGS DO NOT PURPORT TO SHOW ALL EXISTING UTILITIES. EXISTING UTILITIES SHALL BE VERIFIED IN FIELD PRIOR TO INSTALLATION OF ANY NEW LINES.
22. CONTRACTOR IS RESPONSIBLE FOR COMPLYING TO THE STANDARDS AND SPECIFICATIONS OF THE CAPE FEAR PUBLIC UTILITY AUTHORITY WITH REGARDS TO MATERIALS AND INSTALLATION OF THE WATER AND SEWER LINES.
23. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES OR UTILITIES BY OTHERS AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE WATER AND SEWER DEPARTMENT. THE EXISTENCE OF UNDISCOVERED AND UNLOCATED UTILITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. A COPY OF WHICH MUST BE PURCHASED FROM THE MAIN OF THE CONTRACTOR AND KEPT AT THE JOB SITE AT ALL TIMES. REFERENCE TO RECORD SHALL MEAN THE CURRENT STANDARDS AND/OR SPECIFICATIONS OF THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION.
24. ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM WITH THE CURRENT SPECIFICATIONS AND STANDARDS OF THE AMERICAN WATER WORKS ASSOCIATION (AWWA). THE AWWA CONSTRUCTION STANDARDS ARE SET FORTH IN THEIR CONSTRUCTION SPECIFICATIONS AND STANDARD FOR WATER AND SANITARY SERVICES. FACILITIES. A COPY OF WHICH MUST BE PURCHASED FROM THE MAIN OF THE CONTRACTOR AND KEPT AT THE JOB SITE AT ALL TIMES. REFERENCE TO RECORD SHALL MEAN THE CURRENT STANDARDS AND/OR SPECIFICATIONS OF THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION.
25. THE CONTRACTOR SHALL BE REQUIRED TO EXCAVATE BELOW PLAN GRADE ANY MATERIALS WHICH ARE UNSUITABLE FOR FOUNDATIONS, SUB GRADES, PIPE TRENCH BOTTOMS OR OTHER PURPOSES AND REPLACE THEM WITH AN APPROVED MATERIAL. THE EXISTENCE OF UNDISCOVERED AND UNLOCATED UTILITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. A COPY OF WHICH MUST BE PURCHASED FROM THE MAIN OF THE CONTRACTOR AND KEPT AT THE JOB SITE AT ALL TIMES. REFERENCE TO RECORD SHALL MEAN THE CURRENT STANDARDS AND/OR SPECIFICATIONS OF THE NORTH CAROLINA DEPARTMENT OF TRANSPORTATION.
26. A MINIMUM VERTICAL SEPARATION OF 24" SHALL BE MAINTAINED BETWEEN SANITARY SEWER & WATER LINES AND A FULL JOINT OF WATER LINE PIPE SHALL BE CENTERED WHERE WATER LINE CROSSES OVER SANITARY SEWER. WHERE CLEARANCE IS LESS THAN 18" BUT GREATER THAN 12", SANITARY SEWER SHALL BE PRESSURE TESTED DUCTILE IRON PIPE 10' FROM WATER MAIN. WHERE WATER LINE CROSSES UNDER SANITARY SEWER, 18" MINIMUM CLEARANCE MUST BE MAINTAINED, AND SANITARY SEWER SHALL BE PRESSURE TESTED DUCTILE IRON PIPE 10' FROM WATER MAIN.
27. ALL PVC WATERLINES SHALL HAVE BURIED WITH THE PIPE # 12 COATED ELECTRIC WIRE AND BROUGHT UP INTO THE METER BOXES.
28. THE CONTRACTOR SHALL PROVIDE A SURVEY AS-BUILT RECORD DRAWING OF THE SANITARY SEWER SYSTEM AND THE WATER DISTRIBUTION SYSTEM IN ACCORDANCE WITH THE REQUIREMENTS OF THE CAPE FEAR PUBLIC UTILITY AUTHORITY.

BACKFLOW PREVENTION NOTES:

1. THERE SHALL BE NO TAPS, PIPING BRANCHES, UNAPPROVED BYPASS PIPING, HYDRANTS, FIRE DEPARTMENT CONNECTION POINTS, OR OTHER WATER-USING APPURTENANCES CONNECTED TO THE SUPPLY LINE BETWEEN ANY WATER METER AND ITS UTILITY DEPARTMENT REQUIRED BACKFLOW PREVENTERS.
2. EACH UTILITY DEPARTMENT REQUIRED BACKFLOW PREVENTER ASSEMBLY IS REQUIRED TO BE TESTED BY AN APPROVED CERTIFIED TESTER PRIOR TO PLACING THE WATER SYSTEM INTO SERVICE.
3. REDUCED PRESSURE PRINCIPLE BACKFLOW PREVENTION ASSEMBLIES SHALL BE INSTALLED ABOVE-GROUND WITHIN INSULATED ENCLOSURE PER UTILITY DEPARTMENT REQUIREMENTS (HEATED TO 40 DEG F MIN.). ENCLOSURE TO INCLUDE DRAIN PORT(S) FOR DISCHARGE WATER. SEE DETAIL.



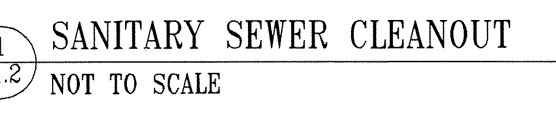
UTILITY LEGEND

UNDERGROUND ELECTRIC	---	UGE	---	UGE
UNDERGROUND TELEPHONE	---	UT	---	UT
WATER LINE	---	W	---	W
SEWER LINE	---	SS	---	SS
GAS LINE	---	G	---	G
ELECTRIC METER				
CLEAN OUT				
GAS METER				
DOMESTIC/IRRIGATION METER				
YARD HYDRANT				

NOTE: 1. CONTRACTOR TO BE RESPONSIBLE FOR VERIFYING & APPLYING FOR UTILITY SERVICE WITH EACH UTILITY COMPANY PRIOR TO STARTING CONSTRUCTION.

UTILITY SERVICE NOTES:

- Water Service**
General Contractor to provide and install a 1" type "K" copper water line from existing main to building.
Contact: Cape Fear Public Utility Authority
Telephone: 910-332-6550
- Electric Service**
"POWER CO." to provide underground 120/208/3 phase service. General Contractor to provide and install two 4" dia. conduit w/ secondary wire to utility company point of connection.
Contact: Telephone:
- Telephone Service**
"TELEPHONE CO." to provide new underground service. General Contractor to provide and install a 4" dia. PVC conduit w/ pull wire from phone panel board to utility company point of connection.
Contact: Telephone:
- Sanitary Sewer**
General Contractor to provide and install a 4" Schedule 40 PVC from existing sewer to last clean out outside of building. General Contractor to provide a 6" cast iron under building slab. (min. 1% slope). Provide clean outs every 60' (typical).
Contact: Cape Fear Public Utility Authority
Telephone: 910-332-6550
- Gas Service**
"GAS CO." to provide new gas line from existing connection to gas meter.
Contact: Telephone:



TRIANGLE SITE DESIGN

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RALEIGH, NC 27609
(919) 553-6570
LICENSE #0-0619

SEAL:

NORTH CAROLINA
PROFESSIONAL
SEAL
2449
ENGINEER
MATTHEW E. LONER

AutoZone

STORE NUMBER: 6486
3913 Oleander Drive
(US Hwy 76)
Wilmington, NC
New Hanover County

OWNER/DEVELOPER:
AUTOZONE DEVELOPMENT, LLC
ATTN. WADE DAVIS
123 S. FRONT STREET
3RD FLOOR
MEMPHIS, TN 38103-2107
PHONE: (901) 495-8701
FAX: (901) 495-8969
wade.davis@autozone.com

For Bidding & Contractor Information Contact:
F.W. Dodge Plan Room Tel: (615) 884-1017

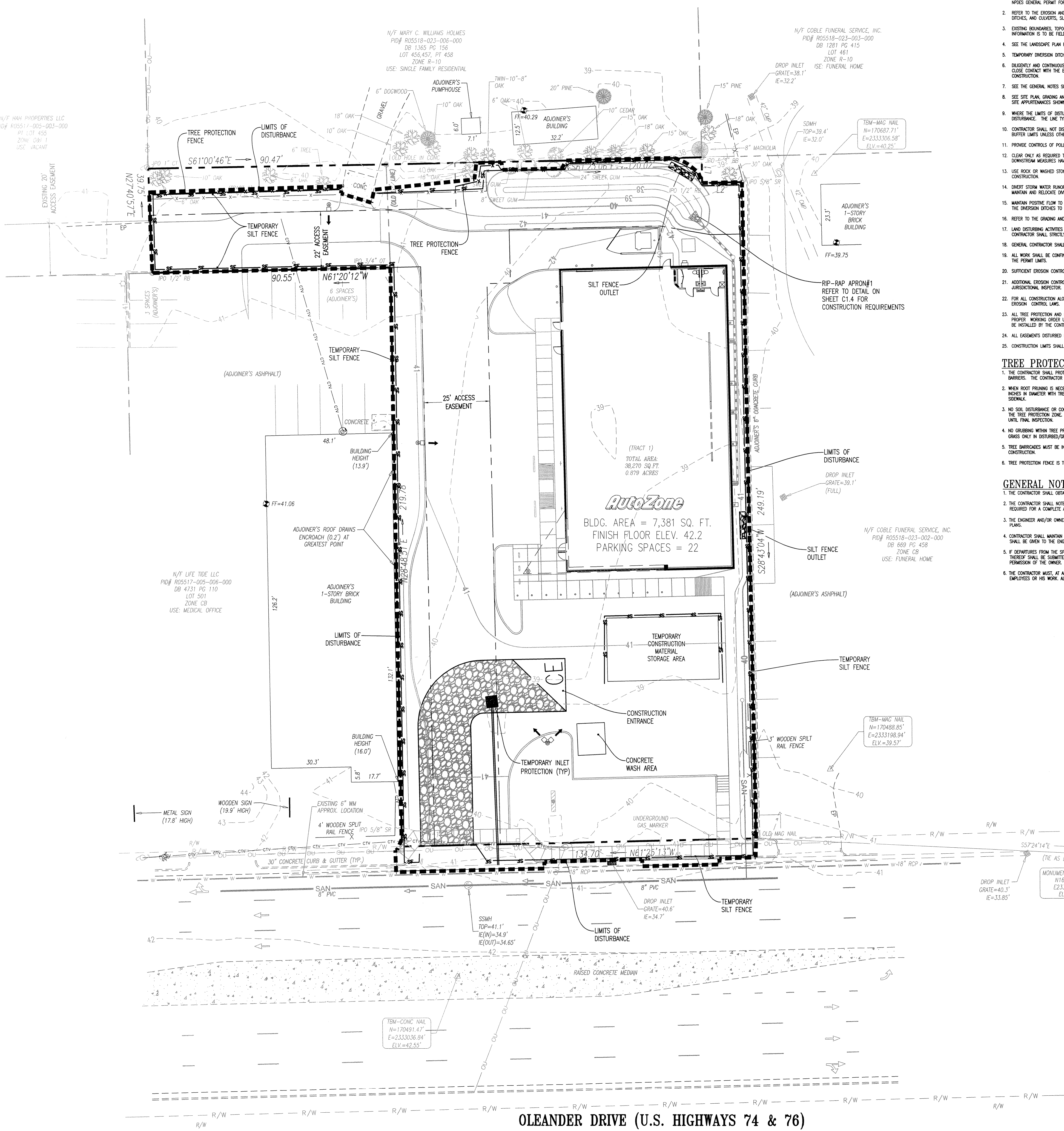
REVISIONS:

LAYOUT COORD:	MEL
PLANNING MGR.	MEL
DRAWING BY:	MEL
DATE:	SEPTEMBER 29, 2015
JOB NUMBER:	010048
TITLE:	UTILITY PLAN

SHEET NUMBER:

C1.2

COMMENTS:



SEDIMENT & EROSION CONTROL NOTES:

1. THE EROSION AND SEDIMENTATION CONTROL MEASURES (BMPs) WERE DESIGNED USING THE NORTH CAROLINA (NCEM) & CITY OF WILMINGTON REQUIREMENTS AND SHALL BE INSTALLED ACCORDINGLY. CONTRACTOR SHALL PERFORM ALL ACTIVITIES IN STRICT COMPLIANCE WITH THE NORTH CAROLINA NPDES GENERAL PERMIT FOR STORM WATER DISCHARGES FROM LARGE AND SMALL CONSTRUCTION ACTIVITIES (GENERAL PERMIT).
2. REFER TO THE EROSION AND SEDIMENT CONTROL DETAIL SHEETS FOR EROSION CONTROL DETAILS AND DESIGN TABLES FOR SEDIMENT BASINS, DIVERSION DITCHES, AND CURBS, SLOPE GRADING, RIP-RAP APRONS AND OTHER EROSION CONTROL MEASURES.
3. EXISTING BOUNDARIES, TOPOGRAPHY, 100-YR FLOODPLAIN, UTILITY AND ROAD INFORMATION TAKEN FROM AN EXISTING CONDITIONS SURVEY. ALL EXISTING INFORMATION IS TO BE FIELD VERIFIED BY THE CONTRACTOR.
4. SEE THE LANDSCAPE PLAN FOR LOCATIONS OF PROPOSED PLANTINGS AND FINAL STABILIZATION.
5. TEMPORARY DIVERSION DITCHES AND BERM SHALL BE MAINTAINED AS THE SITE IS BROUGHT TO GRADE.
6. DILIGENTLY AND CONTINUOUSLY MAINTAIN ALL EROSION CONTROL DEVICES AND STRUCTURES TO MINIMIZE EROSION. THE CONTRACTOR SHALL MAINTAIN CLOSE CONTACT WITH THE EROSION CONTROL INSPECTORS SO THAT PERIODIC INSPECTIONS CAN BE PERFORMED AT APPROPRIATE STAGES OF CONSTRUCTION.
7. SEE THE GENERAL NOTES SHEET AND THE GRADING AND DRAINAGE PLAN FOR OTHER NOTES REGARDING GRADING ACTIVITIES.
8. SEE SITE PLAN, GRADING AND DRAINAGE PLAN, UTILITY PLAN, PLANTING PLAN AND OTHER NOTES FOR DETAILED DESIGN INFORMATION OF PERMANENT SITE APPURTENANCES SHOWN ON THIS SHEET.
9. WHERE THE LIMITS OF DISTURBANCE AND TEMPORARY FENCE (OF 5'-FT. OR 10'-FT. LIMITS ARE ADJACENT, THE TEMPORARY FENCE LINE IS THE LIMITS OF DISTURBANCE. THE LINE TYPES ARE SHOWN SEPARATED FOR ILLUSTRATIVE PURPOSES ONLY.
10. CONTRACTOR SHALL NOT DISTURB ANY EXISTING VEGETATIVE GROUND COVER OR TREES OUTSIDE OF THE LIMITS OF DISTURBANCE OR WITHIN ANY REQUIRED BUFFER LIMITS UNLESS OTHERWISE NOTED OR ILLUSTRATED.
11. PROMOTE CONTROLS OF POLLUTANTS, INCLUDING, BUT NOT LIMITED TO DUST CONTROL, DE-WATERING, SOLID WASTE DISPOSAL, AND HAZARDOUS MATERIALS.
12. CLEAR ONLY AS REQUIRED TO INSTALL EROSION AND SEDIMENTATION CONTROL MEASURES. MASS CLEARING AND GRUBBING CAN BEGIN ONLY AFTER ALL DOWNSTREAM MEASURES HAVE BEEN INSTALLED.
13. USE ROCK OR WASHED STONE TO BRING CONSTRUCTION EXIT TO GRADE. IMPLEMENT WEED WASHES AS NECESSARY THROUGHOUT ALL PHASES OF CONSTRUCTION.
14. DRYEST STORM WATER RUNOFF OFF THE FACE OF THE SEDIMENT BASIN SLOPES USING DIVERSION DITCHES AND SLOPE DRAINS. CONTRACTOR SHALL MAINTAIN AND RELOCATE DIVERSION DITCHES AND SLOPE DRAINS TO ENSURE STORM WATER RUNOFF DOES NOT EXPOSE THE FACE OF FINAL SLOPES.
15. MAINTAIN POSITIVE FLOW TO THE SEDIMENT BASIN THROUGHOUT ALL PHASES OF CONSTRUCTION. PLACE EXCAVATED SOILS ALONG DOWNSTREAM EDGE OF THE DIVERSION DITCHES TO PROVIDE ADDITIONAL CAPACITY.
16. REFER TO THE GRADING AND DRAINAGE PLAN FOR FINAL SITE AND PAVEMENT GRADES AND ELEVATIONS OF THE PROPOSED STORM SEWER SYSTEMS.
17. LAND DISTURBING ACTIVITIES SHALL NOT COMMENCE UNTIL APPROX. 10 TO 50 HAS BEEN RECEIVED BY GOVERNING AUTHORITIES. THE GENERAL CONTRACTOR SHALL STRICTLY ADHERE TO THE APPROVED EROSION AND SEDIMENT CONTROL DRAWINGS DURING CONSTRUCTION OPERATIONS.
18. GENERAL CONTRACTOR SHALL COMPLY WITH ALL STATE AND LOCAL ORDINANCES THAT APPLY.
19. ALL WORK SHALL BE CONFINED TO PERMIT LIMITS SHOWN ON PLANS. UNLESS OTHERWISE NOTED, THE SITE PLAN PROPERTY LIMITS SHALL BE CONSIDERED THE PERMIT LIMITS.
20. SUFFICIENT EROSION CONTROL PRACTICES MUST BE INSTALLED AND MAINTAINED TO RETAIN SEDIMENT WITHIN THE BOUNDARIES OF THE SITE.
21. ADDITIONAL EROSION CONTROL MEASURES OR SILT BARRIERS TO BE PLACED AS SHOWN AND/OR DIRECTED BY THE PROJECT ENGINEER AND/OR LOCAL JURISDICTIONAL INSPECTOR.
22. FOR ALL CONSTRUCTION ALONG AND/OR ACROSS WATERWAYS, BANK PROTECTION AND STABILIZATION SHALL BE REQUIRED AS PER LOCAL JURISDICTIONAL EROSION CONTROL LAWS.
23. ALL TREE PROTECTION AND EROSION CONTROL MEASURES SHALL BE IN PLACE PRIOR TO COMMENCING CONSTRUCTION AND SHALL BE MAINTAINED IN PROPER WORKING ORDER UNTIL ALL DISTURBED AREAS ARE STABILIZED AND GROUND COVER IS ESTABLISHED. CONSTRUCTION ENTRANCE PADS SHALL BE INSTALLED BY THE CONTRACTOR AT CONSTRUCTION ACCESS POINTS PRIOR TO LAND DISTURBANCE.
24. ALL EASEMENTS DISTURBED MUST BE DRESSED AND GROSSED TO CONTROL EROSION IN ACCORDANCE WITH EASEMENT PLATS PRIOR TO ACCEPTANCE.
25. CONSTRUCTION LIMITS SHALL NOT BE EXCEEDED WITHOUT THE APPROVAL OF CITY OF WILMINGTON OR NCEM.

TREE PROTECTION NOTES:

1. THE CONTRACTOR SHALL PROTECT ALL TREES AND SHRUBS OUTSIDE OF CUT/FILL LINES. IN ADDITION TO THOSE THAT RECEIVE TREE/SHRUB PROTECTION BARRIERS, THE CONTRACTOR IS ALSO REQUESTED TO SAVE ALL OTHER EXISTING TREES AND SHRUBS WHERE POSSIBLE.
2. WHEN ROOT PRUNING IS NECESSARY, CUT ROOTS CLEANLY USING A DISC TREENCHER AND IMMEDIATELY COVER ALL ROOT CUT SURFACES LARGER THAN TWO INCHES IN DIAMETER WITH TREE WOUND DRESSING. USE PLYWOOD FORMS WHEN TREE ROOTS ARE ADJACENT TO PROPOSED CURB & GUTTER OR SIDEWALK.
3. NO SOIL DISTURBANCE OR COMPACTION, CONSTRUCTION MATERIALS, TRAFFIC, BURIAL PITS, TREENCHING OR OTHER LAND DISTURBING ACTIVITY ALLOWED IN THE TREE PROTECTION ZONE. TREE BARRIERS MUST BE INSTALLED BEFORE ANY DEMOLITION, GRADING OR CONSTRUCTION BEGINS, AND NOT REMOVED UNTIL FINAL INSPECTION.
4. NO GRUBBING WITHIN TREE PROTECTION ZONE. LEAVE SOIL AND LEAF LITTER UNDISTURBED. SUPPLEMENT WITH 1-2 INCHES OF MULCH. RE-SEED WITH GRASS ONLY IF DISTURBED/GRADED AREAS.
5. TREE BARRIERS MUST BE INSTALLED BEFORE ANY DEMOLITION, CLEARING, GRADING OR CONSTRUCTION BEGINS AND IS NOT TO BE REMOVED UNTIL AFTER CONSTRUCTION.
6. TREE PROTECTION FENCE IS TO BE LOCATED 1 FOOT PER TREE DIAMETER MIN AWAY FROM THE TREE.

GENERAL NOTES:

1. THE CONTRACTOR SHALL OBTAIN AND PAY FOR ALL INSPECTIONS, CERTIFICATIONS, EQUIPMENT, ETC. THAT MAY BE REQUIRED.
2. THE CONTRACTOR SHALL NOTE THAT THE DRAWINGS ARE SCHEMATIC IN NATURE AND DO NOT SHOW EVERY OFFSET, TRANSITION, FITTING, ETC. THAT MAY BE REQUIRED FOR A COMPLETE AND WORKING SYSTEM.
3. THE ENGINEER AND/OR OWNER DISMISSES ANY ROLE IN THE CONSTRUCTION MEANS AND METHODS ASSOCIATED WITH THE PROJECT AS SET FORTH IN THESE PLANS.
4. CONTRACTOR SHALL MAINTAIN AN "AS BUILT" SET OF DRAWINGS TO RECORD THE EXACT LOCATION OF ALL PIPING PRIOR TO CONCEALMENT. DRAWINGS SHALL BE GIVEN TO THE ENGINEER UPON COMPLETION OF THE PROJECT WITH A COPY OF THE TRANSMITTAL LETTER TO THE OWNER.
5. IF DEPARTURES FROM THE SPECIFICATIONS OR DRAWINGS ARE DEEMED NECESSARY BY THE CONTRACTOR, DETAILS OF SUCH DEPARTURES AND REASONS THEREFOR SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW. NO DEPARTURES FROM THE CONTRACT DOCUMENTS SHALL BE MADE WITHOUT THE WRITTEN PERMISSION OF THE OWNER.
6. THE CONTRACTOR MUST, AT ALL TIMES, KEEP THE PREMISES FREE FROM ACCUMULATIONS OF WASTE MATERIALS OR RUBBISH CAUSED BY HIM, HIS EMPLOYEES OR HIS WORK. ALL DEBRIS SHALL BE REMOVED FROM THE PROJECT SITE ON A DAILY BASIS.

EROSION CONTROL NOTES:

1. TOTAL AREA DISTURBED = 0.8 ACRES.
TOTAL SITE AREA = 0.8 ACRES
SOIL TYPE = URBAN LAND COMPLEX
2. UNLESS OTHERWISE INDICATED, ALL VEGETATIVE AND STRUCTURAL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE CONSTRUCTED AND MAINTAINED IN ACCORDANCE WITH THE MINIMUM STANDARDS AND SPECIFICATIONS OF THE NCEM EROSION AND SEDIMENT CONTROL HANDBOOK & CITY OF WILMINGTON REQUIREMENTS.
3. THE CONTRACTOR SHALL INSTALL AND MAINTAIN THROUGHOUT THE PROJECT CONSTRUCTION ALL EROSION CONTROL MEASURES SHOWN WITHIN THESE PLANS IN ACCORDANCE WITH APPLICABLE NORTH CAROLINA & CITY OF WILMINGTON EROSION AND SEDIMENT CONTROL REGULATIONS.
4. CONSTRUCTION WORK SHALL BE IN COMPLIANCE WITH REGULATIONS OF THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM (NPDES) STORM WATER GENERAL PERMIT.
5. EROSION AND SEDIMENT CONTROL MEASURES ARE TO BE PLACED PRIOR TO CLEARING AND/OR LAND DISTURBANCE.
6. A COPY OF THE APPROVED EROSION AND SEDIMENT CONTROL PLAN AND PERMIT SHALL BE MAINTAINED ON THE SITE AT ALL TIMES.
7. THE CONTRACTOR SHALL DILIGENTLY AND CONTINUOUSLY MAINTAIN ALL EROSION CONTROL DEVICES AND STRUCTURES TO MINIMIZE EROSION. THE CONTRACTOR SHALL MAINTAIN CLOSE CONTACT WITH THE NCEM & CITY OF WILMINGTON EROSION CONTROL INSPECTOR SO THAT PERIODIC INSPECTIONS CAN BE PERFORMED AT APPROPRIATE STAGES OF CONSTRUCTION.
8. APPROVAL OF THIS PLAN IS NOT AN AUTHORIZATION TO GRADE ADJACENT PROPERTIES. WHEN FIELD CONDITIONS WARRANT OFF-SITE GRADING, PERMISSION MUST BE OBTAINED FROM THE ADJACENT PROPERTY OWNERS. A REVISION PLAN SHOWING OFF-SITE IMPACTS SHOULD BE SUBMITTED AND APPROVED PRIOR TO ANY OFF-SITE GRADING. CONTACT PROJECT ENGINEER AND PROJECT EROSION CONTROL INSPECTOR TO ENSURE ADDITIONAL EROSION CONTROL MEASURES ARE INSTALLED PRIOR TO OFF-SITE GRADING.
9. PRIOR TO COMMENCING LAND DISTURBING ACTIVITIES IN AREAS OTHER THAN THOSE INDICATED ON THESE PLANS INCLUDING, BUT NOT LIMITED TO OFF-SITE, BORROW OR WASTE AREAS, SHADING OR STORAGE AREAS, THE CONTRACTOR SHALL PREPARE AND SUBMIT A SUPPLEMENTARY EROSION CONTROL PLAN TO THE OWNER FOR REVIEW AND TO NCEM & CITY OF WILMINGTON FOR APPROVAL. THE CONTRACTOR SHALL PAY ALL FEES REQUIRED AND SHALL INSTALL NECESSARY MEASURES AT NO SEPARATE PERMIT. THE CONTRACTOR SHALL PROVIDE THE OWNER AND THE ENGINEER A COPY OF THE MARKED PERMIT.
10. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED CONTINUOUSLY, RELOCATED WHEN AND AS NECESSARY, AND SHALL BE CHECKED AFTER EVERY RAINFALL. SEEDING AREAS SHALL BE CHECKED REGULARLY AND SHALL BE WATERED, FERTILIZED, RE-SEED AND MULCHED AS NECESSARY TO OBTAIN A DESIRED STAND OF GRASS.
11. STABILIZATION IS THE BEST FORM OF EROSION CONTROL. ALL DISTURBED AREAS WHICH ARE NOT OTHERWISE STABILIZED SHALL BE TOP SOILED AND SEEDING, TEMPORARILY OR PERMANENTLY IN ACCORDANCE WITH THE NCEM & CITY OF WILMINGTON EROSION CONTROL REGULATIONS. PERMANENT SEEDING AND GRASS ESTABLISHMENT IS REQUIRED PRIOR TO PROJECT COMPLETION AND ACCEPTANCE.
12. WHEN A CRUSHED STONE CONSTRUCTION ENTRANCE HAS BEEN COVERED WITH SOIL OR HAS BEEN PUSHED INTO THE SOIL BY CONSTRUCTION TRAFFIC, IT SHALL BE REPLACED WITH A DEPTH OF STONE EQUAL TO THAT OF THE ORIGINAL APPLICATION.
13. TEMPORARY GRAVEL CONSTRUCTION ENTRANCE SHALL BE REQUIRED AT ALL CONSTRUCTION STAGING AREA ENTRANCES AND ALL CONSTRUCTION ACCESS LOCATIONS INTO NON-PAVED AREA. SIX INCHES OF STONE SHALL BE USED FOR THE TEMPORARY GRAVEL CONSTRUCTION ENTRANCE.
14. ALL DRAINAGE WATERS SHALL BE PROTECTED FROM SEDIMENT. INEFFECTIVE PROTECTION DEVICES SHALL BE IMMEDIATELY REPLACED AND THE INLET CLEARED. FLOWING IS NOT AN ACCEPTABLE METHOD OF CLEANING.
15. SEDIMENT BASINS AND TRAPS, PERIMETER DITCHES, SEDIMENT BARRIERS AND OTHER MEASURES INTENDED TO TRAP SEDIMENT SHALL BE CONSTRUCTED AS A FIRST STEP IN ANY LAND-DISTURBING ACTIVITY AND SHALL BE MADE FUNCTIONAL BEFORE UP-SLOPE LAND DISTURBANCE TAKES PLACE.
16. ALL DISTURBED AREAS ARE TO BE DOWN TO APPROVED SEDIMENT CONTROL MEASURES AT ALL TIMES DURING LAND DISTURBING ACTIVITIES AND DURING SITE DEVELOPMENT UNTIL FINAL STABILIZATION IS COMPLETED.
17. DURING DE-WATERING OPERATIONS, WATER SHALL BE PUMPED INTO AN APPROVED FILTERING DEVICE PRIOR TO DISCHARGING TO RECEIVING OUTLET.
18. THE CONTRACTOR SHALL INSPECT ALL EROSION CONTROL MEASURES PERIODICALLY AND AFTER EACH RAINFALL-PRODUCING EVENT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF THE EROSION CONTROL DEVICES SHALL BE MADE IMMEDIATELY.

CONSTRUCTION SEQUENCE:

1. CONTACT CITY OF WILMINGTON AT () 48 HOURS BEFORE ANY LAND DISTURBANCE OCCURS.
2. CONTRACTOR SHALL ENSURE THAT ALL NECESSARY PERMITS HAVE BEEN RECEIVED AND THAT THOSE REQUIRED TO BE DISPLAYED ON-SITE ARE AVAILABLE.
3. CONTRACTOR SHALL HOLD A PRE-CONSTRUCTION MEETING AT LEAST 48 HOURS BEFORE THE ACTUAL START OF CONSTRUCTION. CONTACT CITY INSPECTOR.
4. CONTRACTOR SHALL CONTACT CAROLINA ONE CALL AND ALL ENTITIES HAVING BURIED UTILITIES AT LEAST 72 HOURS PRIOR TO DIGGING OR AS DIRECTED AT THE PRE-CONSTRUCTION MEETING.
5. CONTRACTOR SHALL INSTALL TEMPORARY CONSTRUCTION ENTRANCES AT EACH POINT OF ACCESS TO STORAGE AND CONSTRUCTION AREAS.
6. INITIAL PHASE SEDIMENT AND EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH PLANS BEFORE STARTING WORK INCLUDING SILT FENCE, INLET PROTECTION, DIVERSION DITCHES, TREE PROTECTION, ETC., CLEARING ONLY AS NECESSARY FOR THE INSTALLATION OF THESE MEASURES. CONTRACTOR SHALL INCLUDE A MAIN GAGES AND LOG BOOK CONTAINING THE INSPECTION RECORDS FOR THE SITE.
ALL DEMOLITION DEBRIS IS TO GO TO AN APPROVED LANDFILL OR PERMITTED FACILITY.
CONTRACTOR TO USE PARKING AREAS ADJACENT TO THE PROPOSED BUILDING FOR LAY-DOWN/STAGING AREAS - MAINTAIN THESE AREAS SO THAT SEDIMENT WASH RUNOFF WILL BE DIRECTED TO EROSION CONTROL MEASURES. INITIAL SILT FENCE AROUND STAGING AREA AS SHOWN.
7. CONTRACTOR SHALL DILIGENTLY AND CONTINUOUSLY MAINTAIN ALL EROSION CONTROL DEVICES AND STRUCTURES.
ALL APPLICABLE GAS CONTROL MEASURES MUST BE MAINTAINED UNTIL PERMANENT VEGETATION IS ESTABLISHED.
8. STOCKPILE RIP-RAP ON SITE AND INSTALL ALL EROSION CONTROL MEASURES WHERE THE WORK IS OCCURRING. ADDITIONAL EROSION CONTROL MEASURES MAY BE REQUIRED BY THE ENGINEER OR UPON RECOMMENDATION OF INSPECTION PERSONNEL AS CONDITIONS WARRANT.
9. ALL DISTURBED AREAS SHALL HAVE GROUND STABILIZATION BY THE CONTRACTOR ACCORDING TO THE CHART ON THIS SHEET.
10. STABILIZE SITE AS AREAS ARE BROUGHT UP TO FINISHED GRADE.
11. UPON COMPLETION OF THE INSTALLATION AND WITH APPROVAL OF CITY OF WILMINGTON, ALL EROSION CONTROL DEVICES SHALL BE REMOVED AND DISPOSED OF IN AN ACCEPTABLE MANNER. EACH SECTION SHALL BE SEEDER AT EARLIEST POSSIBLE DATE AFTER INSTALLATION OF PIPE, STRUCTURE, OR ACCESS. COORDINATE WITH THE EROSION CONTROL INSPECTOR PRIOR TO REMOVAL OF EROSION CONTROL MEASURES.
12. ONCE GROUND COVER IS FIRMLY ESTABLISHED, ALL TEMPORARY EROSION CONTROL MEASURES SHALL BE REMOVED WITH THE APPROVAL OF INSPECTOR.
13. ALL EROSION CONTROL MEASURES SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE N.C. EROSION & SEDIMENT CONTROL PLANNING AND DESIGN MANUAL, NCEM, U.S. DEPT. OF AGRICULTURE, AND CITY STANDARDS.

GROUND COVER REQUIREMENTS:

THE ANGLE FOR GRADED SLOPES AND FILLS SHALL BE NO GREATER THAN THE ANGLE THAT CAN BE RETAINED BY VEGETATIVE COVER OR OTHER ADJACENT EROSION-CONTROL DEVICES OR STRUCTURES. IN ANY EVENT, SLOPES LEFT EXPOSED MUST BE COVERED BY THE PERMIT FOR THIS PROJECT AND/OR THE PERMIT FROM ANOTHER ACTIVE CITY OF WILMINGTON OR NCEM. IF A PERMITTED SITE THAT IS APPROPRIATE FOR THE INTENDED USE (I.E. APPLICABLE MINING REQUIREMENTS). NO OFF-SITE ACTIVITIES SHALL OCCUR UNTIL THE PERMIT FOR THIS PROJECT HAS BEEN MODIFIED TO INCLUDE THE AFFECTED AREAS OR IT HAS BEEN VERIFIED WITH THE EROSION CONTROL INSPECTOR THAT THE OFF-SITE AREAS ARE COVERED UNDER ACTIVE APPLICABLE CITY OF WILMINGTON OR NCEM PERMITS.

NOTE TO CONTRACTOR:

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AND THE EROSION CONTROL INSPECTOR OF THE LOCATION OF ANY OFF-SITE BORROW SOURCE OR OFF-SITE WASTE MATERIAL DISPOSAL SITE. OFF-SITE DISTURBANCES COUNT TOWARDS THE OVERALL PROJECT DISTURBED AREA AND MUST BE COVERED BY THE PERMIT FOR THIS PROJECT AND/OR THE PERMIT FROM ANOTHER ACTIVE CITY OF WILMINGTON OR NCEM. IF A PERMITTED SITE THAT IS APPROPRIATE FOR THE INTENDED USE (I.E. APPLICABLE MINING REQUIREMENTS). NO OFF-SITE ACTIVITIES SHALL OCCUR UNTIL THE PERMIT FOR THIS PROJECT HAS BEEN MODIFIED TO INCLUDE THE AFFECTED AREAS OR IT HAS BEEN VERIFIED WITH THE EROSION CONTROL INSPECTOR THAT THE OFF-SITE AREAS ARE COVERED UNDER ACTIVE APPLICABLE CITY OF WILMINGTON OR NCEM PERMITS.

EROSION CONTROL NOTES:

REFER TO DETAIL SHEETS C1.4 FOR CONSTRUCTION DETAILS OF THE PROPOSED EROSION CONTROL MEASURES TO BE USED IN THE DEVELOPMENT OF THIS SITE.

REFER TO SHEET C1.4 FOR TEMPORARY AND PERMANENT SEEDING SCHEDULES FOR PROVIDING GROUND COVER FOR THE DEVELOPMENT.

EROSION CONTROL LEGEND

REFER TO DETAIL SHEET FOR CONSTRUCTION REQUIREMENTS

- CONSTRUCTION ENTRANCE
- RIP RAP APRON
- TEMPORARY INLET PROTECTION
- HORSESHOE INLET PROTECTION
- TEMPORARY SILT FENCE OUTLET
- TEMPORARY CHECK DAM
- TEMPORARY SILT FENCE
- TEMP. DIVERSION DITCH

EROSION CONTROL NARRATIVE:

AUTOZONE, WILMINGTON, NC
CONTRACTOR SHALL FIRST INSTALL THE CONSTRUCTION ENTRANCE. THE CONTRACTOR SHALL THEN MOBLIZE ON SITE AND INSTALL THE TEMPORARY EROSION CONTROL DEVICES INCLUDING SILT FENCE, INLET PROTECTION, TREE PROTECTION FENCE AND OTHER DEVICES IN ACCORDANCE WITH THE PLANS (CLEARING ONLY AS NECESSARY TO INSTALL THESE ITEMS). BEGIN GRADING, CLEARING AND SITE GRADING OPERATIONS. STABILIZATION OF EXPOSED STRUCTURES IS REQUIRED IMMEDIATELY AFTER INSTALLATION. THE ON-SITE STORM SEWER SYSTEM CAN NOW BE INSTALLED. IMMEDIATELY AFTER AN INLET IS INSTALLED, INLET PROTECTION SHALL BE PROPERLY INSTALLED ON THE STRUCTURE. THE ON-SITE SANITARY SEWER AND WATER SYSTEMS MAY ALSO BE INSTALLED ONCE THE SITE IS BROUGHT UP TO GRADE. THE SITE PAD WILL BE PREPARED FOR THE PROPOSED BUILDING. THE ROUGH GRADE WILL THEN BE ESTABLISHED FOR THE SITE. INSTALLATION OF CURB AND GUTTER WILL THEN BE PERFORMED. BASE STONE WILL THEN BE PLACED AND THE GRADING. ALL DISTURBED AREAS WILL BE DRESSED AND SEEDING. REMOVE ALL INLET PROTECTION FROM STORM STRUCTURES WHEN PAVING IS TO BEGIN. PAVING AND STORMING WILL THEN BE COMPLETED. ALL UNCOMPLETED WILL BE COMPLETED. THE ON-SITE STORM SEWER SYSTEM SHALL BE CLEANED OF ANY ACCUMULATED SEDIMENT WHICH SHALL BE DISPOSED OF IN A LAWFUL MANNER. ALL ACCUMULATED SEDIMENT BEHIND SILT FENCE AND OTHER SEDIMENT DEVICES SHALL BE REMOVED AND DISPOSED OF IN A LAWFUL MANNER. ACCORDING TO THE GROUND STABILIZATION REQUIREMENTS ON THIS SHEET, REMOVE ALL REMAINING SEDIMENT CONTROL MEASURES FROM THE SITE. CONTRACTOR SHALL MAINTAIN THE LENGTH OF TIME BETWEEN INITIAL LAND DISTURBANCE AND FINAL VEGETATION STABILIZATION OF THE SITE.
THE PROPOSED EROSION AND SEDIMENT CONTROL MEASURES ARE INTENDED TO TRAP ANY STORMWATER RUNOFF FROM THE CONSTRUCTION SITE AND DETAIN IT LONG ENOUGH FOR SEDIMENT AND POLLUTANTS TO SETTLE OUT OF THE STORMWATER BEFORE DISCHARGE. VARIOUS EROSION CONTROL MEASURES ARE USED TO PREVENT POLLUTANT-LADEN STORMWATER RUNOFF FROM FLOWING ONTO ADJACENT PROPERTIES.

TRIANGLE SITE DESIGN

CONSULTANT:
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(919) 553-6570
LICENSE #P-0619

SEAL:

NORTH CAROLINA PROFESSIONAL ENGINEER
No. 2445
Matthew E. Linder

AutoZone

STORE NUMBER: 6486
3913 Oleander Drive
(US Hwy 76)
Wilmington, NC
New Hanover County

OWNER/DEVELOPER:
AUTOZONE DEVELOPMENT, LLC
ATTN: WENDY DAVIS
123 S. FRONT STREET
3RD FLOOR
MEMPHIS, TN 38103-2107
PHONE: (901) 495-8701
FAX: (901) 495-8969
wade.davis@autozone.com
For Bidding & Contractor Information Contact:
F.W. Dodge Plan Room Tel: (615) 884-1017

REVISIONS:

LAYOUT COORD: MEL
PLANNING MGR. MEL
DRAWING BY: MEL
DATE: SEPTEMBER 29, 2015
JOB NUMBER: 010048
TITLE: EROSION CONTROL PLAN
SHEET NUMBER: C1.3
COMMENTS:

OPERATION & MAINTENANCE PROVISIONS (INFILTRATION)

BMP element: The entire BMP	Potential problem: Trash/debris is present	How to remediate the problem: Remove the trash/debris
The perimeter of the infiltration basin	Areas of bare soil and/or erosive gullies have formed	Regrade the soil if necessary to remove the gully, and then plant a ground cover and water until it is established. Provide time and a one-time fertilizer application
The inlet device: pipe or swale	The pipe is clogged (if applicable)	Unclog the pipe. Dispose of the sediment off-site.
	The pipe is cracked or otherwise damaged	Replace the pipe
	Erosion is occurring in the swale (if applicable)	Regrade the swale if necessary to smooth it over and provide erosion control devices such as reinforced turf matting or rip-rap to avoid future problems with erosion
The forebay	Sediment has accumulated and reduced the depth to 75% of the original design depth	Search for the source of the sediment and remedy the problem if possible. Remove the sediment and dispose of it in a location where it will not cause impacts to streams or the BMP. Replace any media that was removed in the process. Revegetate disturbed areas immediately.
	Erosion has occurred or rip-rap is displaced	Provide additional erosion protection such as reinforced turf matting or rip-rap if needed to prevent future erosion problems
	Weeds are present	Remove the weeds, preferably by hand. If pesticides are used, wipe them on the plants rather than spraying
The main treatment area	A visible layer of sediment has accumulated	Search for the source of the sediment and remedy the problem if possible. Remove the sediment and dispose of it in a location where it will not cause impacts to streams or the BMP. Replace any media that was removed in the process. Revegetate disturbed areas immediately.
	Water is standing more than 5 days after a storm event	Replace the top few inches of filter media and see if this corrects the standing water problem. If so, revegetate immediately. If not, consult an appropriate professional for a more extensive repair
	Weeds and noxious plants are growing in the main treatment area	Remove the plants by hand or by wiping them with pesticide (do not spray)
The embankment	Shrubs or trees have started to grow on the embankment	Remove shrubs or trees immediately
	An annual inspection by an appropriate professional shows that the embankment needs repair	Make all needed repairs
The outlet device	Clogging has occurred	Clean out the outlet device. Dispose of the sediment off-site
	The outlet device is damaged	Repair or replace the outlet device
The receiving water	Erosion or other signs of damage have occurred at the outlet	Contact the NC Division of Water Quality 401 Oversight Unit at 919-733-1788

OPERATION AND MAINTENANCE

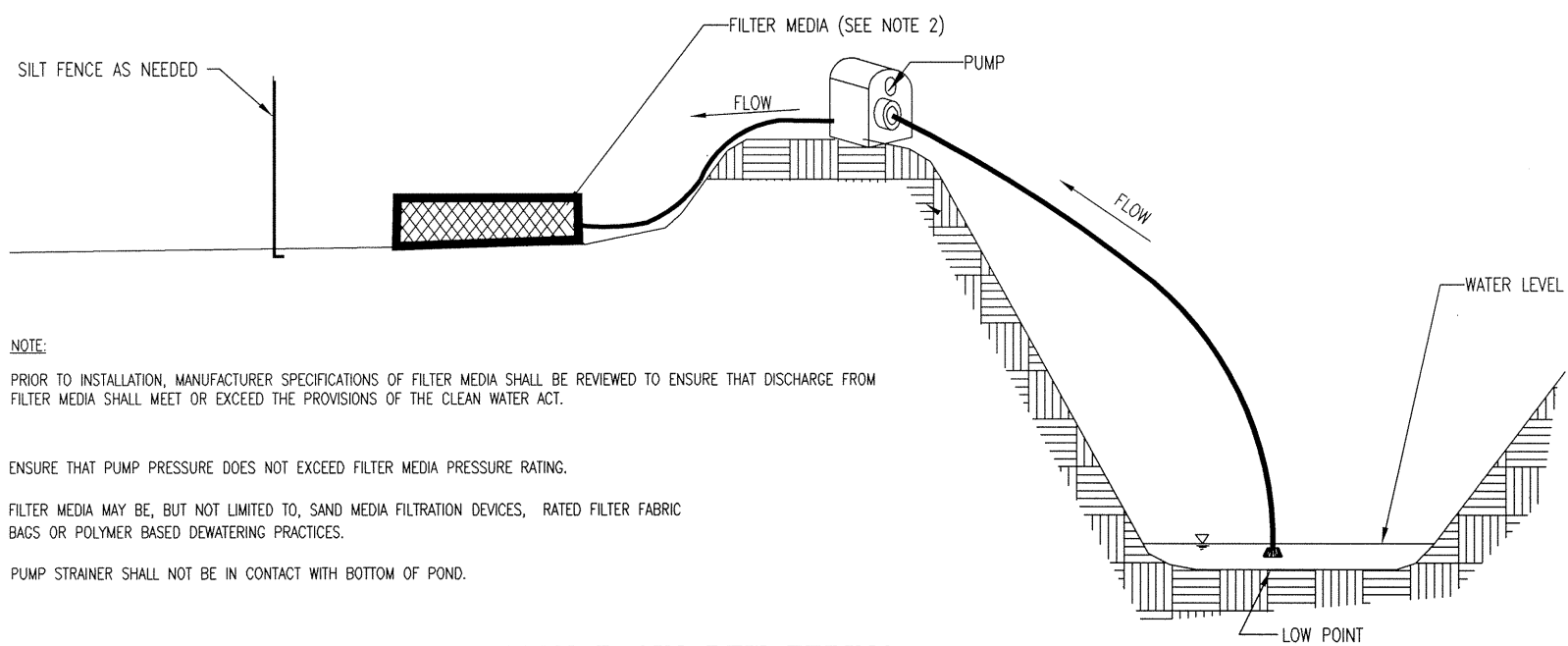
North Carolina storm water rules require quarterly inspections by the regulating agency of infiltration areas as a minimum. More frequent inspections by the land owner or system operator are strongly encouraged to ensure the proper operation of infiltration areas.

- A. Rainfall Event
1. Inspect the basin after every rainfall event greater than 1.5 inches.
- B. Monthly Inspection
1. Inspect the basin monthly
 2. Check the infiltration area side slopes; remove trash and repair eroded areas before the next rainfall event.
 3. Check the vegetative and rock filters for sediment accumulation, erosion and proper operation of the flow spreader mechanism and repair as necessary.
- C. Quarterly Inspection
1. Inspect the collection system (i.e. catch basin, pipes and grass swales) for proper functioning. Clear accumulated trash from basin grates and basin bottoms. Check piping for obstructions.
 2. Check pond inlet pipes for undercutting, replace rip-rap and repair broken pipes.
 3. Reseed grassed swales, including the vegetated filter if applicable, twice a year as necessary. Repair eroded areas immediately.
- D. Six Month Inspection
1. Remove accumulated sediment from the bottom of the outlet structure or other areas where accumulated sediment is noted.
 2. Inspect the embankment taking note of any wet areas where water may be seeping through the soil.
- E. General Inspection
1. Maximum grass height is to be 6 in.
 2. No woody vegetation shall be allowed to grow in the infiltration area.
 3. Debris shall be removed from blocking the inlet and outlet structures and from areas of potential clogging.
 4. Periodic removal of dead vegetation shall be accomplished.
 5. All components of the infiltration system must be kept in good working order.

GRASS SOD NOTE:
GRASS SOD SHALL BE EITHER HYBRID BERMUDA GRASS OR CENTPEDE THAT HAS BEEN GROWN IN A SOIL THAT IS FREE OF AN IMPERMEABLE LAYER (SUCH AS CLAY).

NOTES:

1. ALL INFILTRATION BASINS SHALL HAVE AN ACCESS EASEMENT CONNECTING TO A DEDICATED PUBLIC RIGHT OF WAY.
2. ALL DRAINAGE AREAS TO AN INFILTRATION BASIN ARE TO BE STABILIZED PRIOR TO INSTALLATION OF SOILS OR GRASS SOD.
3. THE DRAINAGE AREA OF THE INFILTRATION BASIN WILL BE CAREFULLY MANAGED TO REDUCE THE SEDIMENT LOAD TO THE SAND FILTER SYSTEM.
4. THE WATER LEVEL IN THE MONITORING WELL(S) WILL BE RECORDED ONCE A MONTH AND AFTER EVERY STORM EVENT GREATER THAN 1.5 INCHES.
5. IMMEDIATELY AFTER THE INFILTRATION BASIN IS ESTABLISHED, THE VEGETATION WILL BE WATERED TWICE WEEKLY IF NEEDED UNTIL PLANTS BECOME ESTABLISHED (COMMONLY SIX WEEKS)
6. NO PORTION OF THE INFILTRATION BASIN WILL BE FERTILIZED AFTER THE INITIAL FERTILIZATION THAT IS REQUIRED TO ESTABLISH THE VEGETATION.
7. THE VEGETATION IN AND AROUND THE BASIN WILL BE MAINTAINED AT A HEIGHT OF APPROXIMATELY SIX INCHES.



INFILTRATION BASIN DEWATERING

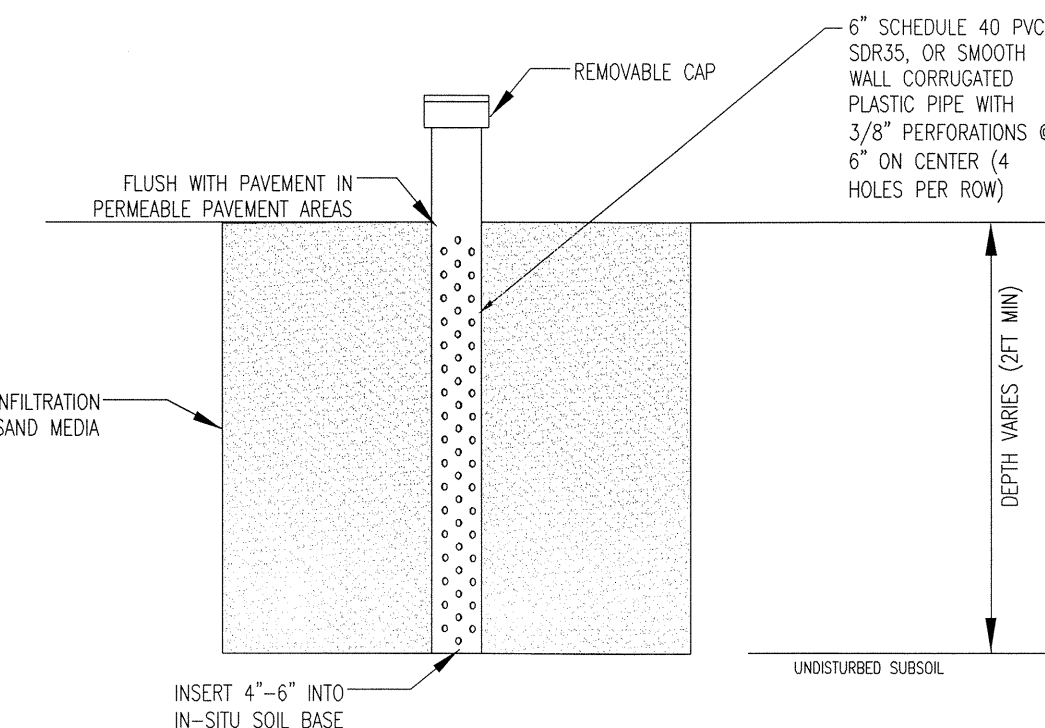
TOP OF EMBANKMENT = 40.0
25-YEAR STORM ELEVATION = 39.75
10-YEAR STORM ELEVATION = 39.71
1-YEAR STORM ELEVATION = 39.52
1.5" STORM ELEVATION = 38.55
BOTTOM OF BASIN = 38.0

INFILTRATION STAGE/STORAGE TABLE

STAGE (FT)	ELEVATION (FT)	CONTOUR AREA (SQ FT)	INCREMENTAL STORAGE (CF)	TOTAL STORAGE (CF)
0.0	38.0	160	0	0
0.5	38.5	400	1.55	1.55
1.0	39.0	655	261	397
1.5	39.5	925	383	790
2.0	40.0	1210	532	1322

SEASONAL HIGH WATER TABLE = VARIES (TYPICALLY 2FT BELOW EXISTING GRADE PER SOILS REPORT)

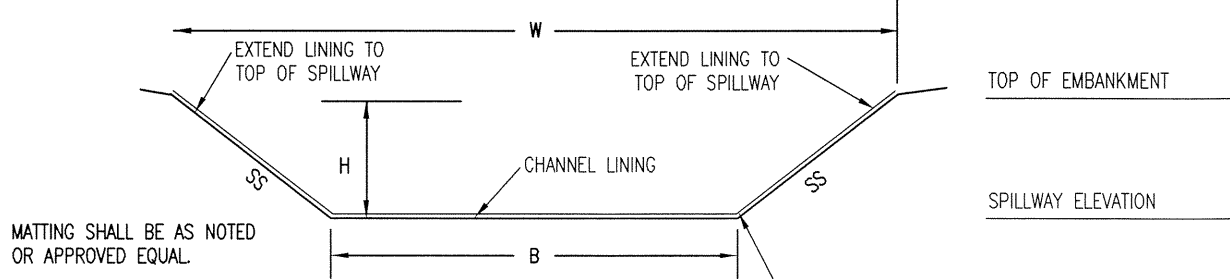
INFILTRATION BASIN DATA



OBSERVATION WELL

INFILTRATION BASIN TYPICAL SECTION NTS

SPILLWAY	FLOW Q(25)	LONG. SLOPE(%)	H	B	W	SS	CHANNEL LINING	TOP OF EMBANKMENT ELEVATION	SPILLWAY ELEVATION
INFILTRATION SPILLWAY	1.64 CFS	0.5%	0.5'	5'	8'	3:1	STRAW WITH NET MAG 15/75	40.0	39.5



GRASS SPILLWAY DETAIL

FILTER MIXTURE (FROM TOP)

1"-0" OF CLEAN ASTM C-33 SAND (MEDIUM AGGREGATE SAND WITH A SIZE RANGE OF 0.02-INCH TO 0.04-INCH DIAMETER) (4" MIN. DEPTH)

LAYER OF GEOTEXTILE PERMEABLE FILTER FABRIC (MIRAFI N-SERIES OR EQUIVALENT)

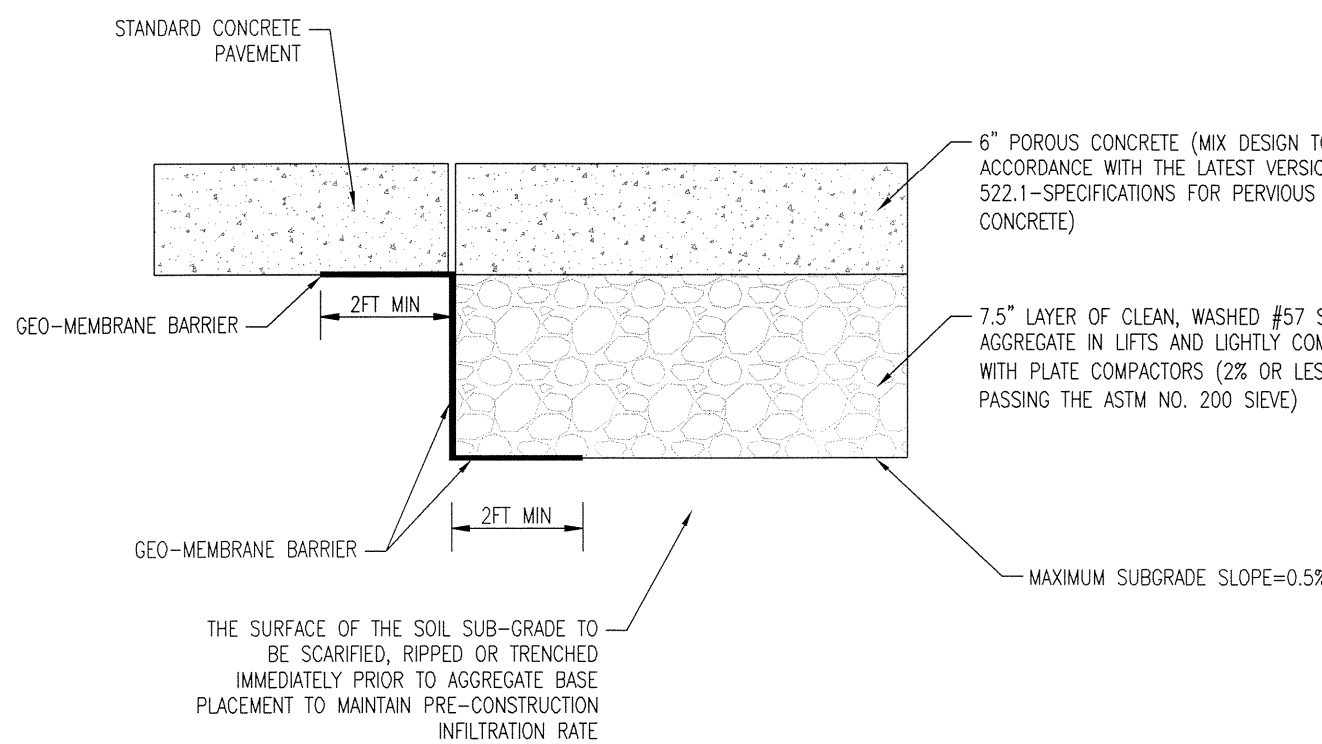
1'-0" MIN. UNDISTURBED IN-SITU SOIL

STORMWATER MANAGEMENT INFILTRATION POND

WATERSHED:	CAPE FEAR RIVER		
DESIGN STORM:	1.5" - COASTAL COUNTY		
INFILTRATION POND DESIGN SUMMARY			
DRAINAGE AREA TO INFILTRATION:		0.21 ACRES	
TOTAL DESIGN IMPERVIOUS AREA TO INFILTRATION:		0.17 ACRES	
		PRE-DEVELOPED	POST-DEVELOPED POST DEVELOPED THROUGH POND
DRAINAGE AREA:		0.21 AC	0.21 AC
CURVE NUMBER:		80.3	91.0
1-YEAR STORM EVENT:	0.499 CFS	0.716 CFS	0.047 CFS
10-YEAR STORM EVENT:	1.257 CFS	1.482 CFS	1.210 CFS
25-YEAR STORM EVENT:	1.880 CFS	1.893 CFS	1.643 CFS

IMPERVIOUS AREA SUMMARY

TOTAL SITE AREA =	38,270 SF (0.88 AC)
EXISTING IMPERVIOUS AREA =	20,153 SF (0.46 AC TO BE DEMOLISHED)
PROPOSED BUILDING =	7,381 SF
PROPOSED ON-SITE SIDEWALK =	1,523 SF
PROPOSED PERVIOUS CONCRETE =	2,984 SF (11,937 SF TOTAL, 75% PERVIOUS, 25% BUA (TYPE B SOILS))
PROPOSED ON-SITE PAVEMENT =	4,188 SF
TOTAL ON-SITE IMPERVIOUS AREA =	16,076 SF
TOTAL ON-SITE OPEN SPACE =	22,194 SF (0.51 AC)
NET DECREASE IN IMPERVIOUS AREA =	4,077 SF (0.09 AC)
(EXEMPT FROM 25-YEAR PRE/POST ATTENUATION REQUIREMENT)	



POROUS CONCRETE PAVEMENT SECTION

PERMEABLE PAVEMENT

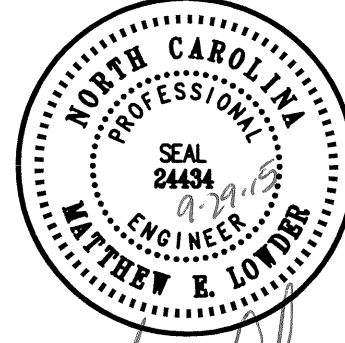
- ACTIVITIES PROHIBITED:
- SANDING
 - RE-SEALING
 - RE-SURFACING
 - POWER WASHING
 - STORAGE OF MULCH OR SOIL
 - STORAGE OF SNOW PILES
 - STORAGE OF HEAVY LOADS
 - APPLICATION OF SALT OR DE-ICERS

EXAMPLE PERMEABLE PAVEMENT SIGN

OPERATION & MAINTENANCE PROVISIONS (PERMEABLE PAVEMENT)

BMP element:	Potential problem:	How to remediate the problem:
The perimeter of the permeable pavement	Areas of bare soil and/or erosive gullies	Regrade the soil if necessary to remove the gully, then plant ground cover and water until established
	A vegetated area drains toward the pavement	Regrade the area so that it drains away from the pavement, then plant ground cover and water until established
The surface of the permeable pavement	Trash/debris present	Remove the trash/debris
	Weeds	Do not pull the weeds (may pull out media as well). Spray them with a systemic herbicide such as glyphosate and then return within the week to remove them by hand. (Another option is to pour boiling water on them or steam them)
	Sediment	Vacuum sweep the pavement
	Rutting, cracking, or damaged structure	Consult and appropriate professional
Observation well	Water present more than five days after a storm event	Clean out clogged underdrain pipes. Consult an appropriate professional for clogged soil subgrade.
Educational sign	Missing or is damaged	Replace the sign

SEAL:



REVISIONS:

LAYOUT COORD: MEL

PLANNING MGR. MEL

DRAWING BY: MEL

DATE: SEPTEMBER 29, 2015

JOB NUMBER: 010048

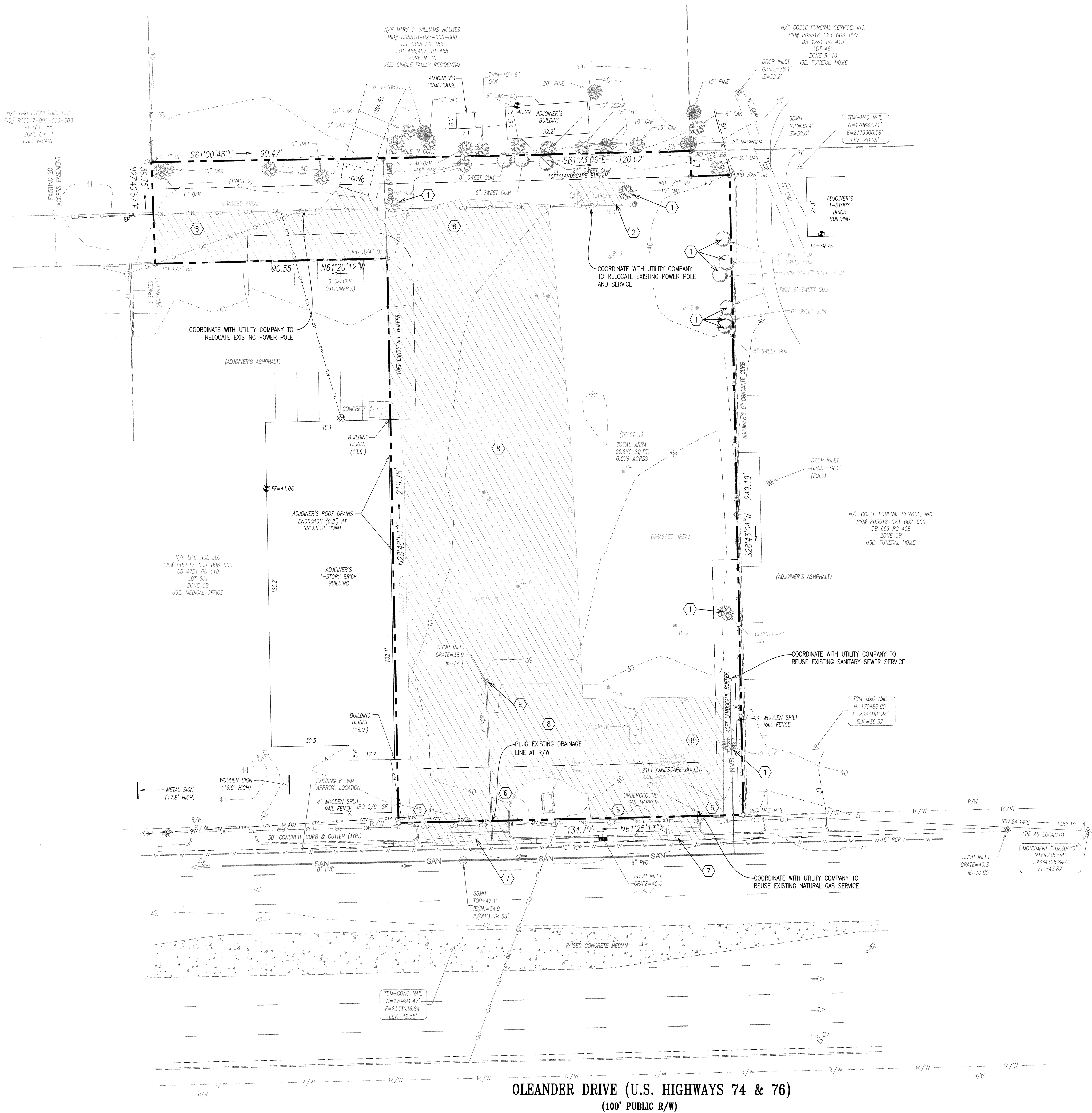
TITLE:

STORMWATER
MANAGEMENT DETAILS

SHEET NUMBER:

C1.5

COMMENTS:



TRIANGLE SITE DESIGN

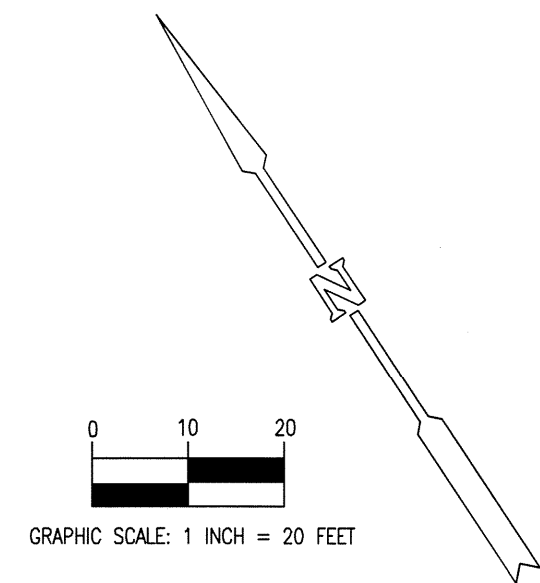
CONSULTANT:
TRIANGLE SITE DESIGN, PLLC
4006 BARRETT DR, STE 203
RALEIGH, NC 27609
(919) 553-6570
LICENSE #P-0619

CAUTIONARY NOTE

THE LOCATIONS OF ALL EXISTING UNDERGROUND UTILITIES SHOWN ON THIS DRAWING ARE BASED ON ABOVE GROUND EVIDENCE OR INFORMATION GATHERED DURING THE FIELD SURVEY PORTION OF THIS PROJECT. AS SUCH, THIS INFORMATION IS SPECULATIVE IN NATURE ONLY AND SHOULD NOT BE CONSTRUED AS FACT. THERE MAY ALSO BE OTHER EXISTING UNDERGROUND UTILITIES FOR WHICH NO ABOVE GROUND EVIDENCE WAS OBSERVED. THE EXACT LOCATIONS OF THE UNDERGROUND UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO THE START OF ANY AND ALL CONSTRUCTION.

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN-SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH THE SURVEYOR DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.



KEYNOTES

DEMOLITION

- 1 Remove trees and root system
- 2 Remove existing building and foundation
- 3 Remove fence, post, and foundation
- 4 Remove all existing shrubs and debris
- 5 Remove existing sidewalks
- 6 Remove existing concrete curb
- 7 Remove existing driveway approaches
- 8 Remove existing pavement
- 9 Remove existing utilities

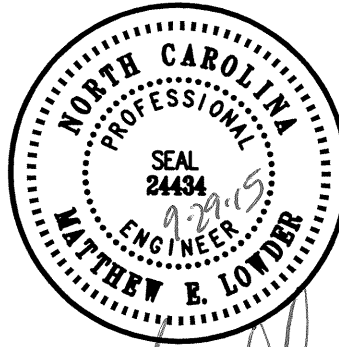
DEMOLITION NOTES

GENERAL DEMOLITION NOTES:

1. ALL WORK TO BE ACCOMPLISHED IN STRICT ACCORDANCE WITH ALL LOCAL ORDINANCES, CITY OR STATE.
2. WITHIN THE SUBJECT PROPERTY, THE INTENT IS TO HAVE A CLEAN, CLEAR SITE, FREE OF ALL EXISTING ITEMS NOTED TO BE REMOVED IN ORDER TO PERMIT THE CONSTRUCTION OF THE NEW PROJECT.
3. ALL ITEMS NOTED TO BE REMOVED BY THE SELLER SHALL BE ACCOMPLISHED PRIOR TO THE CLOSING OF THE REAL ESTATE TRANSACTION. ALL OTHER ITEMS NOTED TO BE REMOVED SHALL BE DONE SO AS PART OF THE CONTRACT FOR GENERAL CONSTRUCTION.
4. REMOVE AND DISPOSE OF ANY SIDEWALKS, FENCES, STAIRS, WALLS, DEBRIS AND RUBBISH REQUIRING REMOVAL FROM THE WORK AREA IN AN APPROVED OFF SITE LANDFILL.
5. THE CONTRACTOR SHALL SECURE ALL PERMITS FOR HIS DEMOLITION AND DISPOSAL OF HIS DEMOLITION MATERIAL TO BE REMOVED FROM THE SITE. THE CONTRACTOR SHALL POST BONDS AND PAY PERMIT FEES AS REQUIRED. BUILDING DEMOLITION CONTRACTOR SHALL BE RESPONSIBLE FOR PERMITS AND DISPOSAL OF BUILDING DEMOLITION DEBRIS.
6. THE DETAILED PLANS MAY NOT REFLECT ALL UTILITIES ON THE SITE OR SURROUNDING STREETS AND PROPERTIES. THE CONTRACTOR SHALL VERIFY LOCATIONS AND EXISTENCE OF UTILITY SERVICES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL CALL "811" AT 1-800-344-2233, 72 HOURS PRIOR TO CONSTRUCTION.
7. THE CONTRACTOR TO REMOVE ALL UTILITIES TO EXISTING STRUCTURES WHETHER SHOWN OR NOT OR ARRANGE FOR THE APPROPRIATE UTILITY COMPANY TO CUT AND CAP SERVICE PRIOR TO THE PROPERTY LINE OR MAIN (AS REQUIRED). ALL SERVICES MAY NOT BE SHOWN ON THIS PLAN.
8. FOR ALL ITEMS NOTED TO BE REMOVED - REMOVE NOT ONLY THE ABOVE GROUND ELEMENTS, BUT ALL UNDERGROUND ELEMENTS AS WELL INCLUDING BUT NOT NECESSARILY LIMITED TO: FOUNDATIONS, GRAVEL FILLS, TREE ROOTS, OLD PIPES, ETC.
9. BACK FILL ALL EXCAVATIONS RESULTING FROM THE DEMOLITION WORK TO MEET THE REQUIREMENTS FOR FILL OUTLINED IN THE GEOTECHNICAL REPORT.
10. THE CONTRACTOR SHALL PROTECT ALL IRON PINS, MONUMENTS AND PROPERTY CORNERS DURING CONSTRUCTION. ANY CONTRACTOR DISTURBED PINS, MONUMENTS, ETC. SHALL BE RESET BY A LICENSED LAND SURVEYOR AT THE EXPENSE OF THE CONTRACTOR.
11. THE CONTRACTOR SHALL RESTORE ANY UTILITY STRUCTURE, PIPES, PAVEMENT, CURBS, SIDEWALKS OR LANDSCAPED AREAS DISTURBED DURING DEMOLITION TO THEIR ORIGINAL CONDITION TO THE SATISFACTION.
12. ALL BUILDINGS, FOUNDATION WALLS AND FOOTINGS INDICATED ON THIS PLAN TO BE REMOVED FROM SITE. CONTRACTOR SHALL SECURE ANY PERMITS AND PAY ALL FEES AND PERFORM CLEARING AND GRUBBING AND DEBRIS REMOVAL PRIOR TO COMMENCEMENT OF CONSTRUCTION OPERATIONS.
13. ASBESTOS AND ANY OTHER HAZARDOUS MATERIAL SHALL BE REMOVED BY THE GENERAL CONTRACTOR USING A LICENSED HAZARDOUS MATERIAL CONTRACTOR.

OLEANDER DRIVE (U.S. HIGHWAYS 74 & 76)
(100' PUBLIC R/W)

SEAL:



AutoZone

STORE NUMBER: 6486
3913 Oleander Drive
(US Hwy 76)
Wilmington, NC
New Hanover County

OWNER/DEVELOPER:
AUTOZONE DEVELOPMENT, LLC
ATTN: WADE DAVIS
123 S. FRONT STREET
3RD FLOOR
MEMPHIS, TN 38103-2107
PHONE: (901) 495-8701
FAX: (901) 495-8969
wade.davis@autozone.com
For Bidding & Contractor Information Contact:
F.W. Dodge Plan Room Tel: (615) 884-1017

REVISIONS:

LAYOUT COORD: MEL
PLANNING MGR. MEL
DRAWING BY: MEL
DATE: SEPTEMBER 29, 2015
JOB NUMBER: 010048
TITLE:

DEMOLITION PLAN

SHEET NUMBER:

D1.0

COMMENTS:

 TRIANGLE SITE DESIGN	
CONSULTANT: TRIANGLE SITE DESIGN, PLLC 4006 BARRETT DR, STE 203 RALEIGH, NC 27609 (919) 553-6570 LICENSE #P-0619	
SEAL: <div style="text-align: center; margin-top: 20px;">  <i>(Signature)</i> </div>	
<div style="font-size: 2em; font-weight: bold; transform: rotate(-90deg); transform-origin: left bottom; white-space: nowrap;">AutoZone</div> <p style="margin-top: 20px;">STORE NUMBER: 6486 3913 Oleander Drive (US Hwy 76) Wilmington, NC New Hanover County</p>	
OWNER/DEVELOPER: AUTOZONE DEVELOPMENT, LLC ATTN: WADE DAVIS 123 S. FRONT STREET 3RD FLOOR MEMPHIS, TN 38103-2107 PHONE: (901) 495-8701 FAX: (901) 495-8969 wade.davis@autozone.com For Bidding & Contractor Information Contact: F.W. Dodge Plan Room Tel: (615) 884-1017	
REVISIONS:	
LAYOUT COORD:	MEL
PLANNING MGR.	MEL
DRAWING BY:	MEL
DATE:	SEPTEMBER 29, 2015
JOB NUMBER:	010048
TITLE:	
LIGHTING PLAN	
SHEET NUMBER:	
LP1.0	
COMMENTS:	

DATA

ZONED: CB – COMMUNITY BUSINESS

SETBACK REQUIREMENTS:
FRONT – 20 FEET
SIDE – 0 FEET
REAR – 10 FEET

MAXIMUM BUILDING HEIGHT: 45 FEET

MAXIMUM NUMBER OF STORIES: 3

MINIMUM LOT WIDTH: 80'

MINIMUM LOT SIZE: 1/2 ACRES

AUTOZONE IS A PERMITTED USE FOR THIS ZONING DISTRICT.

TOTAL AREA

38,270 SQ.FT.
0.879 ACRES

PARKING REQUIREMENTS

GROSS FLOOR AREA OF
ESTABLISHMENT IN S.F.

REQUIRED NUMBER
OF SPACES

LESS THAN 5,000	N/A
5,000 TO 24,499	1
25,000 TO 49,999	2
50,000 TO 99,000	3
100,000	4

FLOOD_ZONE

ACCORDING TO THE FEDERAL EMERGENCY MANAGEMENT AGENCY'S FLOOD INSURANCE RATE MAP, NEW HANOVER COUNTY, NORTH CAROLINA, WITH AN EFFECTIVE DATE OF APRIL 3, 2006, THE PROPERTY SURVEYED AND SHOWN HEREON LIES WITHIN ZONE "X" PER COMMUNITY MAP NO. 3720313700J. CITY OF WILMINGTON, NORTH CAROLINA NUMBER 370171 3137 J.

REFERENCE PLAT

PLAT ENTITLED "ALTA/ACSM LAND TITLE SURVEY FOR MB ONE CAPITAL, LLC, PREPARED BY PATRICK C. BRISTOW LAND SURVEYING, PC, DATED OCTOBER 9, 2013

TAX ASSESSOR PARCEL NO.

R05518--023--001--000

BASIS OF BEARINGS

NORTH CAROLINA STATE PLANE COORDINATES WERE USED AS THE BASIS OF BEARINGS FOR THIS SURVEY.

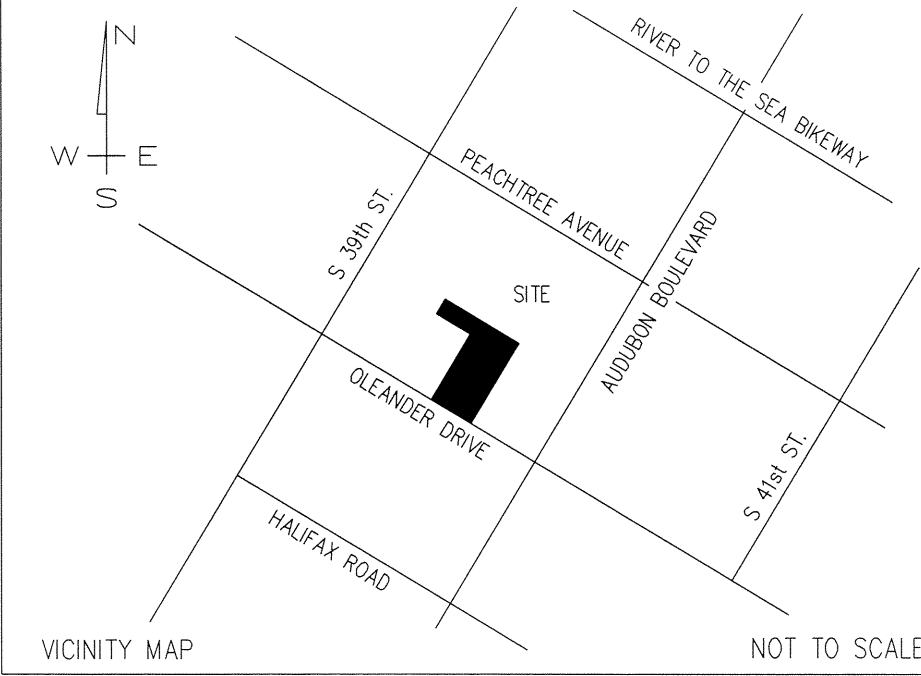
ENCROACHMENTS/PROJECTIONS

(A) ADJOINER'S CONCRETE CURB ENCROACHES 0.5'

TITLE LEGAL DESCRIPTION

MISCELLANEOUS NOTES:

1. NORTH CAROLINA STATE PLANE COORDINATES WERE USED AS THE BASIS OF BEARINGS FOR THIS SURVEY.
2. SUBJECT PROPERTY APPEARS TO DRAIN INTO A PUBLIC RIGHT OF WAY, UNLESS OTHERWISE SHOWN OR NOTED.
3. THERE IS OBSERVABLE EVIDENCE OF EARTH MOVING WORK, BUILDING CONSTRUCTION OF BUILDING ADDITIONS WITHIN RECENT MONTHS ON SUBJECT PROPERTY.
4. REFERENCE DEED: DB 5557 PG 0465
5. THERE ARE NO KNOWN CHANGES IN STREET RIGHT OF WAY LINES, EITHER COMPLETED OR PROPOSED, AND AVAILABLE FROM THE CONTROLLING JURISDICTION.
6. THERE IS OBSERVABLE EVIDENCE OF RECENT STREET OR SIDEWALK CONSTRUCTION OR REPAIRS.
7. THERE IS NO OBSERVABLE EVIDENCE OF SITE USE AS A SOLID WASTE DUMP, SUMP OR SANITARY LAND FILL
8. THERE IS NO OBSERVED EVIDENCE OF THE LOCATION OF CEMETERIES AND BURIAL GROUNDS.
9. MAP PIN: R05518--023--001--000
10. ALL DRIVES/PARKING LOTS ARE ASPHALT PAVING UNLESS OTHERWISE NOTED
11. CURRENT OWNER OF RECORD: INDEPENDENCE GENE LLC
12. DIRECT ACCESS TO SUBJECT PROPERTY VIA PUBLIC RIGHT OF WAY OF OLEANDER DRIVE
13. STREET ADDRESS NOT POSTED AT TIME OF FIELD SURVEY.
14. RECORD DESCRIPTION AS PROVIDED DOES FORM A MATHEMATICALLY CLOSED FIGURE
15. OBSERVED UTILITIES EITHER ENTER SUBJECT PROPERTY FROM PUBLIC RIGHT OF WAY OR THROUGH DEDICATED EASEMENTS, UNLESS OTHERWISE NOTED HEREON
16. BY OBSERVED EVIDENCE ONLY, THE SUBJECT PROPERTY DOES SERVE ADJOINING PROPERTY FOR UTILITIES, DRAINAGE, STRUCTURAL SUPPORT OR INGRESS OR EGRESS
17. NO EVIDENCE OF POTENTIAL WETLANDS WERE OBSERVED ON THE SUBJECT PROPERTY AT THE TIME THE SURVEY WAS CONDUCTED, NOR HAVE WE RECEIVED ANY DOCUMENTATION OF ANY WETLANDS BEING LOCATED ON THE SUBJECT PROPERTY.



SURVEYORS CERTIFICATE

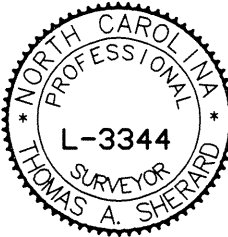
TO:

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2011 MINIMUM STANDARD DETAIL REQUIREMENTS FOR ALTA/ACSM LAND TITLE SURVEYS, JOINTLY ESTABLISHED AND ADOPTED BY ALTA AND NSPS, AND INCLUDES ITEMS 1, 2, 3, 4, 5, 6(B), 7(A), 7(B)(1), 7(C), 8, 9, 10(A), 11(A), 13, 14, 16, 17, 18 AND 21 OF TABLE A THEREOF. THE FIELD WORK WAS COMPLETED ON JANUARY 20, 2015

DATE OF PLAT OR MAP: 1--23--15

BY: T.A. SHERARD, P.L.S.

T.A. Sherard
REGISTRATION NO: L-3344
STATE OF NORTH CAROLINA



THE PROPERTY DESCRIBED AND SHOWN HEREON IS THE SAME PROPERTY DESCRIBED IN [INSERT NAME OF TITLE COMPANY] TITLE COMMITMENT NO. _____ DATED _____, 201__.

SOURCE OF DATA

CITY OF WILMINGTON, N.C.
municode.com

ACCESS NOTE

ACCESS TO SUBJECT PROPERTY VIA PUBLIC
RIGHT-OF-WAY OF OLEANDER DRIVE

LEGEND

BL BUILDING LINE	TEL ☐ TELEPHONE PEDESTAL	CATV ■ CABLE TV PEDESTAL
CL CENTERLINE	EW ELECTRIC METER	TO/BT TOP/BOTTOM CURB
CMP CORRUGATED METAL PIPE	CB ☐ CATCH BASIN	TW/BW TOP/BOTTOM WALL
CT CRIMP TOP	DI ■ DROP INLET	VCP VITRIFIED CLAY PIPE
DE DRAINAGE EASEMENT	ET ELEC TRANS	WV WATER VALVE
EP EDGE OF PAVEMENT	EL ELEVATION	WV WATER VALVE
IPO IRON PIN OLD-○	FF FIRE HYDRANT	CTV CABLE TV
IPS IRON PIN SET-○	GM GAS METER	X FENCE LINE
N&C NAIL & CAP	GV GAS VALVE	FOC FIBER OPTIC CABLE
OT OPEN TOP	LP LIGHT POLE	GAS GAS LINE
RB REBAR	PP POWER POLE	OHP OVERHEAD POWER
ROP REINFORCED CONC PIPE	GP GUY ANCHOR	OHT OVERHEAD TELEPHONE
R/W RIGHT OF WAY	SMH SD MANHOLE	SD STORM DRAIN
SD STORM DRAIN	SSMH SS MANHOLE	SS SANITARY SEWER
SS SANITARY SEWER	SSMH SS MANHOLE	UGP UNDERGROUND POWER
SSE SS EASEMENT	TMH TMH	UGT UNDERGROUND TEL
● B-1 SOIL BORING	CO CLEAN OUT	W WATER LINE

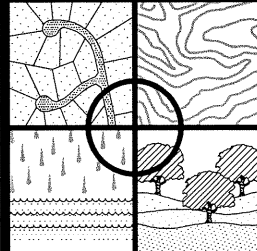
SHEET 1 OF 2

"ALTA / ACSM LAND TITLE SURVEY"
FOR
AUTO ZONE

3913 OLEANDER DRIVE
NEW HANOVER COUNTY
WILMINGTON, NORTH CAROLINA

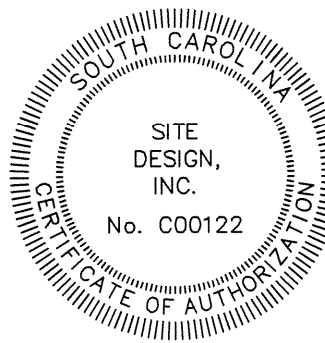
DATE 1/22/2015 FIELD CREW BM/JA DRAWN BY HS CHECKED BY ACJ/DRA

SCALE 1"=20' 20 0 20 40



SITE DESIGN, INC.
CIVIL ENGINEERS - SURVEYORS - LANDSCAPE ARCHITECTS

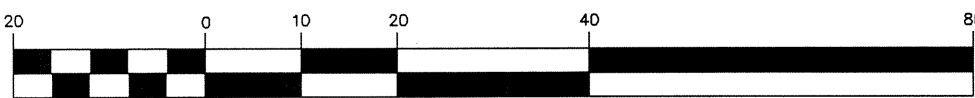
800 E. WASHINGTON ST. STE. B. GREENVILLE, SC 29601
PH: (864)271-0496 FAX: (864)271-0402
www.sitedesign-inc.com



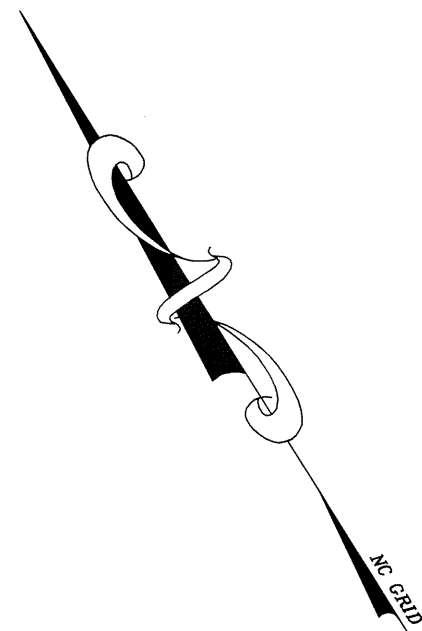
PRELIMINARY
NOT FOR CONSTRUCTION

DATE	REVISION	BY	AP/VD

GRAPHIC SCALE



(IN FEET)
1 inch = 20 ft.

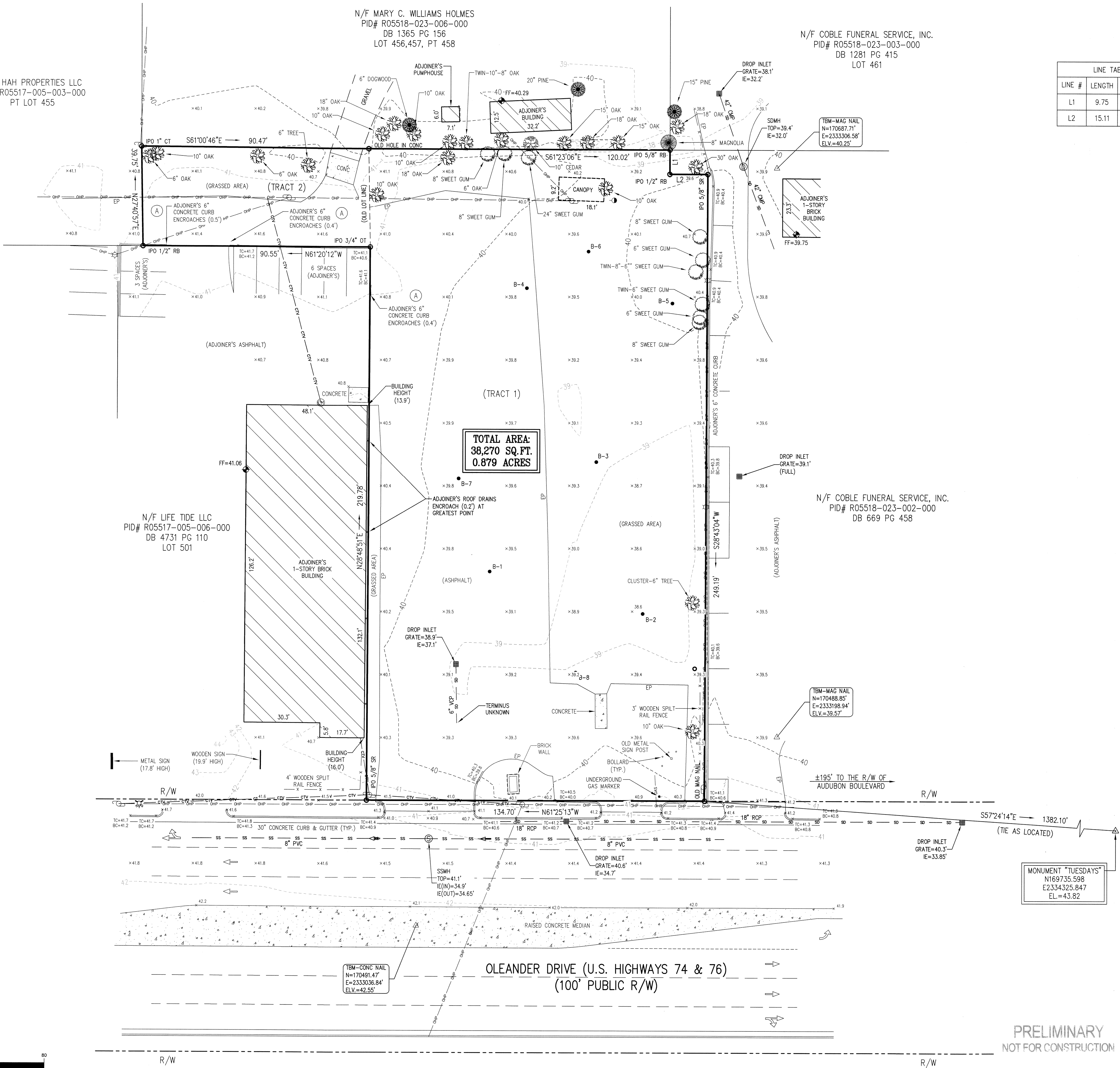
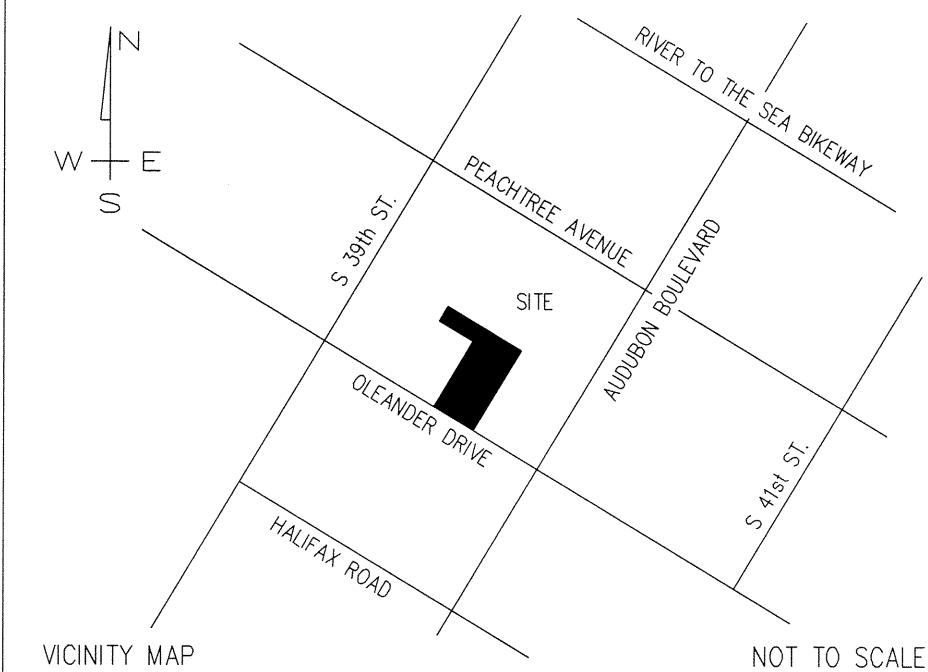


N/F HAH PROPERTIES LLC
PID# R05517-005-003-000
PT LOT 455

N/F MARY C. WILLIAMS HOLMES
PID# R05518-023-006-000
DB 1365 PG 156
LOT 456,457, PT 458

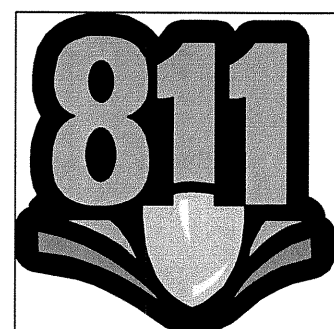
N/F COBLE FUNERAL SERVICE, INC.
PID# R05518-023-003-000
DB 1281 PG 415
LOT 461

LINE TABLE		
LINE #	LENGTH	DIRECTION
L1	9.75	S28°43'20"W
L2	15.11	S61°24'39"E



- TOPO NOTES:
- ELEVATIONS ARE BASED ON (NAVD88)
 - CONTOURS ARE SHOWN AT 1' INTERVALS

CAUTION



THE UTILITIES SHOWN ARE SHOWN FOR THE CONTRACTOR'S CONVENIENCE ONLY. THERE MAY BE OTHER UTILITIES NOT SHOWN ON THESE PLANS. THE ENGINEER ASSUMES NO RESPONSIBILITY FOR THE LOCATIONS SHOWN AND IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATIONS OF ALL UTILITIES WITHIN THE LIMITS OF THE WORK. ALL DAMAGE MADE TO EXISTING UTILITIES BY THE CONTRACTOR SHALL BE THE SOLE RESPONSIBILITY OF THE CONTRACTOR.

LEGEND

BL BUILDING LINE	TEL TELEPHONE PEDESTAL	CATV CABLE TV PEDESTAL
CL CENTERLINE	EL ELECTRIC METER	TC/BC TOP/BOTTOM CURB
CM CORRUGATED METAL PIPE	CB CATCH BASIN	TW/BW TOP/BOTTOM WALL
CT CRIMP TOP	DI DROP INLET	VCP VITRIFIED CLAY PIPE
DE DRAINAGE EASEMENT	ELEC TRANS	WM WATER METER
EP EDGE OF PAVEMENT	ELEV ELEVATION	WV WATER VALVE
IPD IRON PIN OLD	FH FIRE HYDRANT	CTV CABLE TV
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R/W RIGHT OF WAY	SDM SD MANHOLE	SD SANITARY SEWER
SD STORM DRAIN	SSM SS MANHOLE	UGP UNDERGROUND POWER
SS SANITARY SEWER	TMH TELEPHONE MANHOLE	UGT UNDERGROUND TEL
SSE SS EASEMENT	CO CLEAN OUT	W WATER LINE
S-1 SOIL BORING		

SHEET 2 OF 2

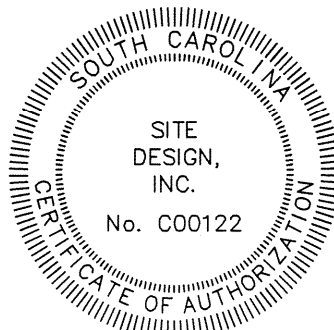
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WILMINGTON, NORTH CAROLINA

DATE	1/22/2015	FIELD CREW	BM/JA	DRAWN BY	HS	CHECKED BY	ACJ/DRA
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SCALE	1"=20'	20	0	20	40
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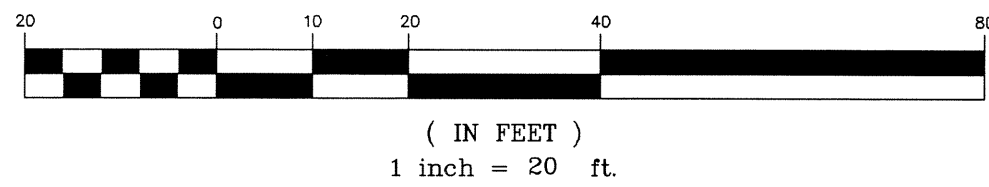
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GRAPHIC SCALE



S150022